

## York Beach Greenway District



### Town of York, Maine

**Most Recently Amended:** November 5, 2024

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#### ENACTMENT BY THE LEGISLATIVE BODY

Date of the vote to enact/amend this Zoning Ordinance: November 5, 2024.

Certified by the Town Clerk:

(signature)

on

(date)

11/13/2024

# TOWN OF YORK, MAINE



## ZONING ORDINANCE AMENDMENT YORK BEACH GREENWAY DISTRICT ARTICLE 10-I CHARACTER-BASED ZONING



**DRAFT**

June 28,  
2024

# PREFACE

## INTRODUCTION

This Preface describes basic concepts of Character-Based zoning and typical Character Districts that may be included in a Character-Based ordinance.

This Preface is not part of the Town of York, Maine Zoning Ordinance or Article 10-I to which it is attached.

## WHAT IS CHARACTER-BASED ZONING?

Character-Based zoning represents a change in the way that the built environment is regulated. This shift is necessary because the conventional, use-based approach to zoning has been shown to be ineffective for regulating diverse, context-sensitive, mixed-use environments. The Town of York is implementing Character-Based regulations specifically for the York Beach Greenway District to help achieve the overarching goals of sustainability, innovation, recreation, economic development, walkability, and improved quality of life.

Character-Based codes foster predictable built results and a high-quality public realm. Unlike conventional land regulations, Character-Based codes use the intended form and character of a place as the organizing principle, or framework, of the code, rather than focusing predominantly on use. They focus on how development relates to the context of the surrounding community or neighborhood, especially the relationships between buildings and lots (the private realm) and the thoroughfare (the public realm).

Character-Based ordinances regulate a series of important elements to create a high-quality place. Those elements include:

At the Building, Lot, & Building Site scale, standards covering:

- Lots
- Building Placement and number
- Building Facades
- Private Frontage
- Building Form

- Building Type
- Use
- Encroachments
- Screens, Streetscreens, & Buffers
- Vehicular Parking
- Non-Building Components (HVAC, pools, etc.)
- Loading Space
- Trash Receptacles
- Private Lot Landscaping & Fencing
- Private Lighting

At the Development Site scale, standards covering:

- Thoroughfares and Internal Drives
- Alleys
- Pedestrian Ways
- Bikeways
- Public Lighting
- Public Planting
- Blocks
- Civic Space

The elements listed above help provide greater predictability both to developers and the Town about the final results of development.

## DISTRICTS

Article 10-I establishes Districts for the York Beach Greenway District (YBGD), established along Short Sands Road, which range from natural to a mixed-use neighborhood center. There is also an Amusement & Entertainment Special District.

The designation of each district is determined first by the character and form, intensity of development, and type of place, and then by the mix of uses within the area.

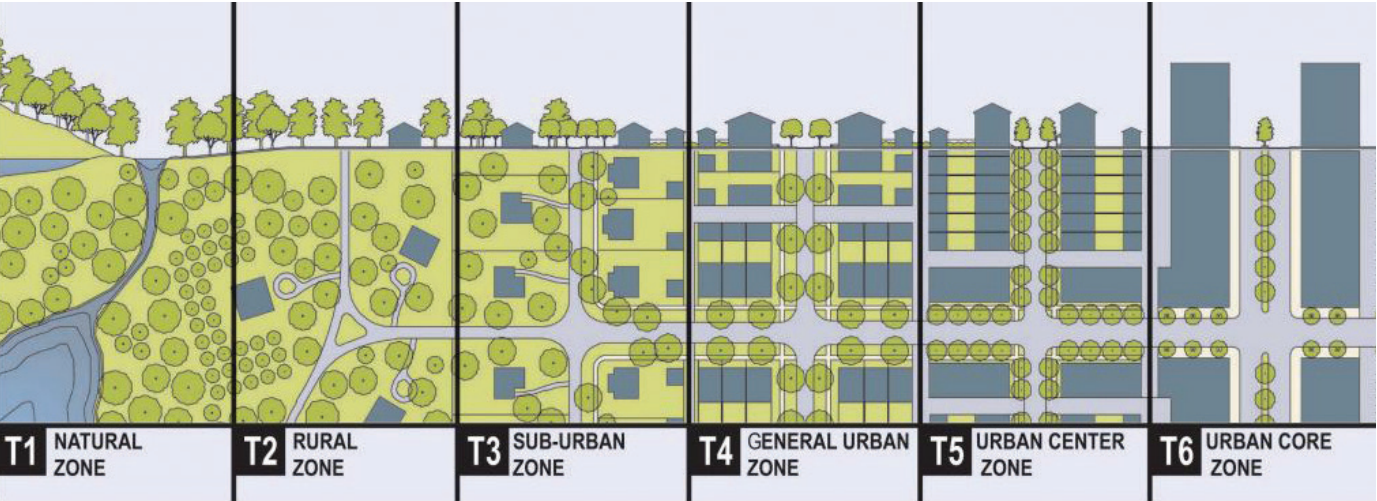
Districts are based on a transect that describes a range of rural-to-urban places. The prototypical transect includes a natural district (T1), a rural district (T2), a suburban district (T3), a general urban district (T4), an urban center district (T5), and an urban core district (T6). See **Illustration P.1 (Rural-To-Urban Transect)** and **Table P.1 (Typical Transect Zone / Character Districts)**.



Article 10-I refers to transect zones as “Character Districts” or “CDs”. Because Article 10-I was prepared for the YBGD, it does not include the rural (CD-2), urban center (CD-5), or urban core (CD-6) districts, as they are

not compatible with the vision or character of the YBGD. Character District names have also been calibrated to reflect the existing character of the Town. The applicable Districts for the YBGD are set out in **Table P.2 (YBGD District Descriptions)** on the following page.

ILLUSTRATION P.1 (RURAL-TO-URBAN TRANSECT)



Sources: Congress for the New Urbanism & Center for Applied Transect Studies




TABLE P.1 (TYPICAL TRANSECT ZONE / CHARACTER DISTRICTS)

TRANSECT ZONE / CHARACTER DISTRICT	DESCRIPTION
T1 or CD-1 (Natural)	This District consists of natural landscape approximating or reverting to a wilderness condition, including lands unsuitable for settlement due to topography, hydrology, or vegetation. This District typically does not contain buildings; however, small Civic Buildings or interpretive centers may be located here, which must be approved by the applicable governing body.
T2 or CD-2 (Rural)	This District consists of sparsely settled, primarily low density single-family detached Residential areas.
T3 or CD-3 (Sub-Urban)	This District consists primarily of a low density single-family detached Residential area in which Houses are the predominant Building Type. It has medium-to-deep front Setbacks and medium-to-wide side Setbacks. Its Thoroughfares and Internal Drives have curbs and may include Sidewalks and/or street trees, and form medium-to-large blocks.
T4 or CD-4 (General Urban)	This District consists of a medium intensity area that has a mix of Building Types and residential, retail, office and other commercial Uses; there are medium, shallow or no front Setbacks and narrow-to-medium side Setbacks; it has variable private landscaping; and it has streets with Curbs, Sidewalks, and Thoroughfare trees that define medium-sized blocks.
T5 or CD-5 (Urban Center)	This District consists of higher density Mixed Use areas. It has a tight network of Thoroughfares and Internal Drives with wide Sidewalks, street lights and regular street spacing, defining medium-sized blocks. Buildings are set close to the Sidewalks.
T6 or CD-6 (Urban Core)	This District consists of the highest density and height with a wide variety of Uses, located near and with connections and access to public transit. This District may have significant Civic Spaces and important Civic Buildings. Its Blocks are defined by Thoroughfares and Internal Drives with curbs, street trees, and wide Sidewalks, with small or no front Setback and no side Setbacks.



# PREFACE

TABLE P.2 (YORK BEACH GREENWAY DISTRICT: DISTRICT DESCRIPTIONS)

District	Description
	<p><b>CD-1 (Natural)</b></p> <p>The CD-1 Natural Character District consists of natural landscape approximating or reverting to a wilderness condition, including lands unsuitable for settlement due to topography, hydrology, or vegetation. This District typically does not contain buildings; however, small Civic Buildings may be located here, which must be approved by the applicable governing body.</p>
	<p><b>CD-3 (Neighborhood)</b></p> <p>The CD-3 Neighborhood Character District consists primarily of a Residential area in which Tiny Homes, Houses, and Duplexes are the predominant Building Types. It has medium-to-deep front Setbacks and medium-to-wide side Setbacks. Its Thoroughfares and Internal Drives have curbs and may include Sidewalks and/or street trees, and form medium-to-large blocks.</p>
	<p><b>CD-4 (Neighborhood Center)</b></p> <p>The CD-4 Neighborhood Center District consists of a medium intensity area that has a mix of Building Types and Residential, Commercial, Office and other Uses; there are medium or shallow front Setbacks and narrow side Setbacks; it has variable private landscaping; and it has Thoroughfares and Internal Drives with Curbs, sidewalks, and street trees that define medium-sized blocks.</p>
<p><b>SD-AE (Amusement &amp; Entertainment)</b></p> <p>The SD-AE Amusement &amp; Entertainment Special District consists of an area designated for between 50% and 100% that meets all requirements of CD-4 and the remaining area, if any (the "A &amp; E Area"), meeting all requirements of CD-4, modified as noted in this Article 10-I and any additional requirements as may be determined and established by the applicable Decision-Making Authority.</p>	

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# SECTION 10-I.1

## DEFINITIONS

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This Section 10-I.1 provides definitions for certain capitalized terms used in this Article 10-I. Other capitalized terms used in this Article 10-I are defined within the text of this Article 10-I and in other Articles of the Town Zoning Ordinance.

Such definitions are integral to this Article 10-I. The meanings of undefined terms shall be determined by the Code Enforcement Officer.

The definitions included in this Article 10-I are applicable only to this Article and shall not be applicable to any other part of the Town Zoning Ordinance.

If a term used in this Article 10-I is defined both in this Article 10-I and in another part of the Town Zoning Ordinance, the definition in this Article 10-I shall be controlling as applied within this Article 10-I. In the event of conflicts between the definitions in this Article 10-I and definitions in other parts of the Town Zoning Ordinance, those of this Article 10-I shall take precedence as applied to this Article 10-I.

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### A

**Accessory Building:** a Building that shares a Lot or Building Site with a Principal Building and that is customarily and clearly incidental and subordinate to such Principal Building. An Accessory Building may or may not have an Accessory Dwelling Unit or be occupied or devoted to one (1) or more Accessory Uses. An Accessory Building may or may not be attached to the Principal Building.

**Accessory Use:** a Use that is customarily and clearly incidental and subordinate to a Principal Use of land or a Structure and is located on the same Lot or Building Site as such Principal Use.

**Adjacent:** having any distance of real property boundary in common with another property, or being separated from the other property boundary by a right-of-way, Alley, Easement, or Civic Space. Not synonymous with abutting.

**Affordable Housing Development:** as defined in Maine Statute, MSRA Title 30-A Section 4364.

**Albedo:** the amount of solar radiation reflected by a surface.

**Alley:** a vehicular way, which is not intended for general traffic circulation, which affords secondary access to Abutting property, and which may or may not be dedicated to public use. An Alley is not a Thoroughfare. See Section 10-I.4.11.3, **Illustration 10-I.4.11.3 (Alley Components)**, and **Table 10-I.4.11.3 (Alley Assembly & Standards)**.

**Animal Boarding:** a Use characterized by housing, feeding, and caring for animals, excluding Animal Hospital / Veterinary Clinic, for a period greater than twenty-four (24) hours for Commercial purposes.

**Animal Hospital / Veterinary Clinic:** premises used by a licensed practitioner of veterinary medicine to treat and manage small household pets and other small animals, such as dogs, cats, rabbits, rodents, birds, reptiles, amphibians, and fish.

**Arcade:** a Private Frontage type which is conventional for Commercial Use, wherein a colonnade supporting

## SECTION 10-I.1: DEFINITIONS

habitable space in one (1) or more upper levels overlaps the Sidewalk, while the Facade at the first level remains at the Frontage Line. See **Table 10-I.4.5.1 (Private Frontage Types)**.

**Artisan Establishment:** any Structure, premises, or establishment in or on which articles of artistic quality or effect or handmade workmanship are produced with or without Retail Sales of such articles. Examples of activities that may be conducted in an Artisan Establishment are surfboard making, candle making, furniture making, glass blowing, weaving, pottery making, woodworking, sculpting, and painting. An Artisan Establishment is not an Industrial Use.

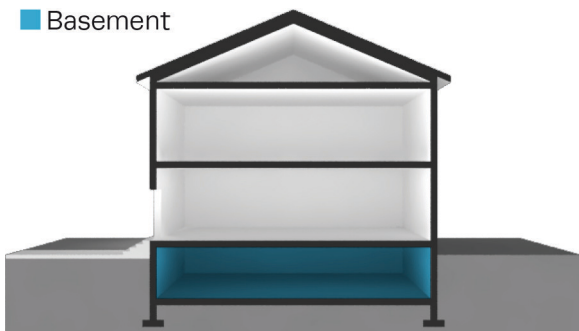
### B

**Bar or Tavern:** an establishment where serving alcoholic beverages to be consumed on the premises is its principal activity.

**Basement:** a portion of a Building having more than half (1/2) of its clear floor-to-ceiling height below the average finished grade of the ground. See **Illustration 10-I.1.B-1 (Basement)**.

**ILLUSTRATION 10-I.1.B-1 BASEMENT**

■ Basement

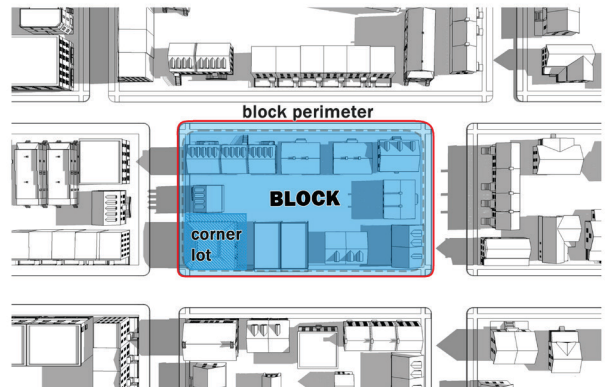
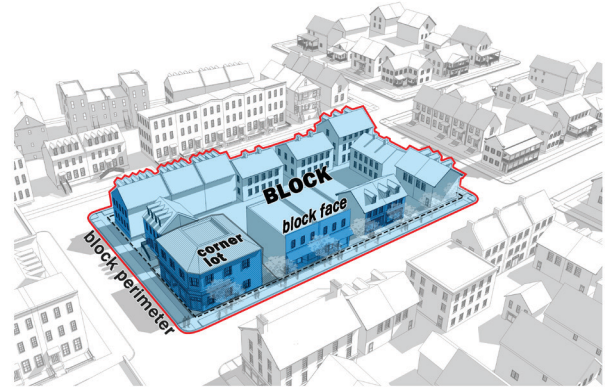


**Bath House:** a Structure containing dressing rooms for Persons using a beach.

**Block:** the aggregate of private Lots and Building Sites, Passages, and alleys, circumscribed by Thoroughfares or Internal Drives, or where applicable, circumscribed

by Thoroughfares or Internal Drives, Civic Space, and/or Sidewalks. See **Illustration 10-I.1.B-2 (Block)**.

**ILLUSTRATION 10-I.1.B-2 BLOCK**



**Botanic Garden:** a garden often with greenhouses for the culture, study, and exhibition of special plants.

**Brew Pub:** a type of Restaurant that also brews and may can alcoholic beverages on-site in accordance with all state and local laws, rules, regulations, and licensing requirements. A Brew Pub is not an Industrial Use.

**Building Element:** any component or part of a Building.

**Building / Lot or Building Site Energy Generation:** on-site conversion of an energy source into mechanical or electrical energy for the use of a single Building. Excess energy may be fed back into the grid, but a connection to the grid is not necessary for individual production.



**Building Height:** as defined in Article 2 of the Town Zoning Ordinance, except that Building Height shall be measured in Stories instead of feet.

**Building Orientation:** a description of the siting of a Building. See **Table 10-I.4.14.1B (Surface-to-Volume Ratio & Building Orientation)** relative to siting of a Building for passive solar purposes.

**Building Site:** a parcel of land that has not been subdivided as a separate Lot, but which is designated by an applicant as a Building Site to be separately built on in a Development Plan, site plan, permit application, or plat, and which accommodates, or is available to accommodate, a Principal Building. See **Illustration 10-I.1.L-2 (Lot or Building Site Line)**. See also "Lot of Record" and "Lot or Building Site".

## C

**Car-Share:** a type of short-term vehicle rental which can substitute for car ownership by providing cars parked within neighborhoods.

**Civic Use:** as defined in Article 2 of this Ordinance.

**Civic Building:** a Building dedicated to Civic Use. See **Table 10-I.4.5.7.1 (Building Types - Summary)** and **Table 10-I.4.5.7.2 (Building Types - Specific Standards)**.

**Civic Space:** one of the several specific types of Open Space designed for Civic Use in accordance with Section 10-I.4.12.3. See **Table 10-I.4.12.3A (Civic Space Types - Summary)** and **Table 10-I.4.12.3B (Civic Space - Specific Standards)**.

**Civic & Public:** Use category collectively referring to the Uses listed as such in **Table 10-I.4.7.1A (Permitted Principal Uses)**.

**Character District:** one of several areas on a Regulating Plan to which certain Character District Development, Building, and Lot and Building Site standards are applied. See **Table 10-I.4.2.1 (District & Civic Space Standards)**.

**Commercial:** operation of or engagement in for-profit activities and endeavors.

**Commercial Building:** See **Table 10-I.4.5.7.1 (Building Types - Summary)** and **Table 10-I.4.5.7.2 (Building Types - Specific Standards)**.

**Commercial Street:** See **Table 10-I.4.11.2.1B (Thoroughfare & Internal Drive Types -Summary)** and **Table 10-I.4.11.2.1C (Thoroughfare & Internal Drive Assemblies & Standards)**.

**Commercial Use(s):** a category of Uses or Use type involving Commercial activities and endeavors, excluding Office Uses, Industrial Uses, Vehicular Uses, Rural & Agricultural Uses, Recreation & Amusement Uses, and Miscellaneous Uses. See **Table 10-I.4.7.1A (Permitted Principal Uses)**, **Table 10-I.4.7.1B (Permitted Accessory Uses)**, and **Table 10-I.4.7.1C (Permitted Temporary Uses)**.

**Commercialfront:** a Private Frontage type wherein the Facade is aligned close to or at the Frontage Line with the Building entrance at or above Sidewalk grade. It may have substantial glazing on the Facade. See **Table 10-I.4.5.1 (Private Frontage Types)**.

**Common Destination:** an area of focused community activity, usually defining the approximate center of a Pedestrian Shed. It may include without limitation one (1) or more of the following: a Civic Space, a Civic Building, a mixed-use center, or a Public Transportation Station, and may act as the social center of a neighborhood.

**Common Yard:** a planted Frontage wherein the Facade is set back substantially from the Frontage Line, and the front yard so created remains unfenced and is visually continuous with abutting yards, supporting a common landscape. See **Table 10-I.4.5.1 (Private Frontage Types)**.

**Community Garden:** Civic Space type for the cultivation of fruits, flowers, vegetables, or ornamental plants by more than one (1) person or family, described as such in

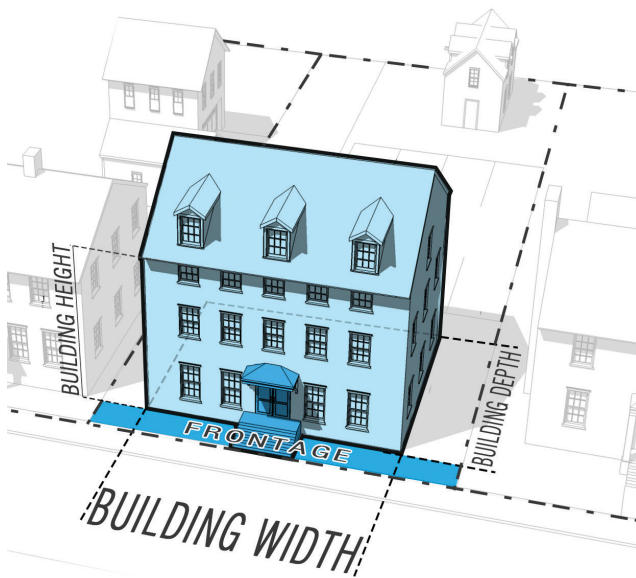
## SECTION 10-I.1: DEFINITIONS

**Table 10-I.4.12.3A (Civic Space Types – Summary)** and **Table 10-I.4.12.3B (Civic Space – Specific Types)**.

**Conditional Use:** as defined in Article 2 of the Town Zoning Ordinance.

**Configuration:** the form of a Building, including its massing, Private Frontage, and height. See **Illustration 10-I.1.C (Configuration)**.

### ILLUSTRATION 10-I.1.C CONFIGURATION



**Corridor:** a lineal geographic system incorporating a Thoroughfare, Greenway, or Open Space.

**Curb:** the edge of the vehicular pavement that may be either raised or flush with a swale to the extent allowed by applicable Thoroughfare design regulations. It usually incorporates or is associated with the drainage system.

### D

**Day Care Center:** a place other than an occupied dwelling, which for compensation provides non-Residential care, protection, and supervision for children, adults, or families on a regular basis away from their primary Residence for fewer than eighteen (18) hours each day with no overnight accommodations.

**Daylighting:** the use of daylight as a primary source of general illumination in a space.

**Decision-Making Authority:** the Town's Code Enforcement Officer, Planning Board, Selectboard, or Legislative Body, as applicable, as authorized to review, make a recommendation with respect to, or take action on a proposal under this Article 10-I or any other provision of the Town Zoning Ordinance.

**Deck:** an approximately horizontal, exterior floor-like platform extending along or over one or more parts of a Building, which platform may be covered or uncovered and enclosed or open excluding, however, a Porch.

**Development / Develop:** commencing, making or planning for man-made changes to land or other real property, and the resulting changes to such land or other real property, whether through Development, re-Development, clearing, excavation, grading, construction, re-construction, modification, subdivision, or re-subdivision, and whether such changes are horizontal, vertical, or subterranean.

**Development Site:** a parcel of land, which either alone or together with one or more other parcels under a common Development scheme, program or plan, is Developed or proposed to be Developed.

**District:** any Character District or Special District.

**Dooryard:** a Private Frontage type intended for ground floor Residential Use, wherein the Facade of the Building is set back a small distance and the Frontage is defined by a low wall, decorative Fence or hedge, creating a small raised, sunken, or at-grade Dooryard consisting of a front garden or patio. The Dooryard shall not be used for public circulation. See **Table 10-I.4.5.1 (Private Frontage Types)**.

**Double Skin:** an additional external layer of a Building Facade that allows air to circulate in the space between the two Facades, providing an extra layer of insulation and air flow.

**Drive:** See **Table 10-I.4.11.2.1B (Thoroughfare & Internal Drive Types –Summary)** and **Table 10-I.4.11.2.1C (Thoroughfare & Internal Drive Assemblies & Standards)**.

**Driveway:** a Vehicular Lane within a Lot or Building Site, often leading to a Garage.

**Duplex:** See **Table 10-I.4.5.7.1 (Building Types – Summary)** and **Table 10-I.4.5.7.2 (Building Types – Specific Standards)**.

## E

**Easement:** a right to use another's real property for a specific purpose not inconsistent with the owner's right to hold the same property.

**Edgeyard:** the yard type that results from a Building being set back from its Lot or Building Site boundaries at the front, rear, and both sides.

**Education or Educational:** a Use category characterized by the process of receiving or giving instruction.

**Elevation:** as related to the exterior walls of a Building, one that is not along a Frontage Line; when not capitalized, "elevation" means the height above a given level or a drawing or design that represents an object or Structure as being projected geometrically on a vertical plane parallel to one of its sides, as the context indicates. See also Facade.

**Encroach:** to break the plane of a vertical or horizontal regulatory limit (e.g. a Setback, the Public Frontage, a right-of-way, a height limit, or a property line).

**Encroachment:** the condition wherein the plane of a vertical or horizontal regulatory limit (e.g. a Setback, the Public Frontage, a right-of-way, a height limit, or a property line) is broken.

**Energy Demand Reduction:** a method to achieve Zero Net Energy goals, whereby Building energy use is reduced by combining traditional site specific building practices with current material technology.

**Enfront:** to place, or the location of, an element along a Frontage, as in "porches Enfront the street."

## F

**Facade:** an exterior wall of a Building that is set along a Frontage Line. Facades define the public realm and are therefore more regulated than the Elevations facing other Lot or Building Site Lines.

**Fast Food Restaurant:** a Restaurant, with or without on-premises seating and with minimal or no table service, which offers to customers who order and receive at a counter from a limited menu food cooked in advance, kept heated, finished and packaged to order, and available for take away or delivery. A Restaurant operated on premises as an Accessory Use to an entertainment, recreation, or amusement business Principal Use is not a Fast Food Restaurant.

**Fence:** a Structure, a device erected to serve as an architectural element, landscape element, Screen, Streetscreen, or physical barrier.

**Fencefront:** a Private Frontage Type, a Frontage wherein the Facade is set back from the Frontage Line, and the front yard so created is separated from the Public Frontage by a Fence. See **Table 10-I.4.5.1 (Private Frontage Types)**.

**Flex Building:** See **Table 10-I.4.5.7.1 (Building Types – Summary)** and **Table 10-I.4.5.7.2 (Building Types – Specific Standards)**.

**Forecourt:** a Private Frontage wherein a portion of the Facade is close to the Frontage Line and the central portion is set back, creating a gathering space for residents or additional shopping or dining area. This type



## SECTION 10–I.1: DEFINITIONS

should be used sparingly and may be used in conjunction with other Private Frontage types. See **Table 10–I.4.5.1 (Private Frontage Types)**.

**Frontage:** the area between a Facade and a Path, Passage, waterbody, Civic Space, or the curb (or if there is no curb, the edge) of the Vehicular Lanes of a Thoroughfare, inclusive of the built and planted components of such area. Frontage is divided into Private Frontage and Public Frontage. See **Illustration 10–I.1.P3 (Private Frontage)**, **Illustration 10–I.1.P4 (Public Frontage)**, and **Table 10–I.4.5.1 (Private Frontage Types)**.

**Frontage Line:** a Lot or Building Site Line bordering a Public Frontage.

**Full Cutoff:** a Luminaire type that does not allow any light to be emitted or reflected above a horizontal plane.

**Full–Service Restaurant:** See Restaurant – Full Service.

### G

**Gallery:** with respect to the Frontage, the Private Frontage type conventional for Commercial Use, wherein the Facade is aligned close to the Frontage Line with an attached cantilevered shed or lightweight colonnade overlapping the Sidewalk. See **Table 10–I.4.5.1 (Private Frontage Types)**.

**Geothermal System:** See “Ground Source Heat Exchange or Geothermal System.”

**Green:** a Civic Space type for unstructured recreation, spatially defined by landscaping rather than Building Frontages. See **Table 10–I.4.12.2A (Civic Space Types – Summary)** and **Table 10–I.4.12.2B (Civic Space – Specific Standards)**.

**Green Roof:** a building roof partially or completely covered with vegetation and soil, or a growing medium, over a waterproofing membrane.

**Greenway:** (a) generally, a corridor encompassing a trail for bicycles and pedestrians. The trajectory of a Greenway should lead through rural as well as urban areas, connecting the countryside to urban Civic Spaces. The landscaping pattern should be appropriate to the location: naturalistic within the countryside, and formal within the neighborhoods. A Greenway should follow the natural trajectory which is transformed to its purpose. (b) with respect to the York Beach Greenway District, such district.

**Ground Source Heat Exchange or Geothermal System:** a process by which the relatively constant temperature below ground is used in building climate control systems as a heat source for heating, or a heat sink for cooling. Also referred to as a “Geothermal System”.

### H

**Heavy Industrial:** Industrial Uses characterized by:

- (a) factory production or industrial yards;
- (b) capital- or energy-intensive machinery or equipment;
- (c) production or involvement of large or complex products or processes;
- (d) research and development activities other than light fabrication or assembly;
- (e) sales to other Businesses;
- (f) use, sale, or production of heavy machinery or equipment;
- (g) generation, causing, discharging, or disseminating any material, noise, smoke, pollution, gas, fumes, dust, odor, heat, glare, or vibration beyond the walls of an enclosed Building situated on the premises, or beyond the boundaries of the premises on which the activity is conducted;
- (h) Heavy Manufacturing; or
- (i) activities or Uses that are potentially:
  - (i) detrimental to the environment,
  - (ii) pose any danger or hazard of fire, explosion, or

other physical hazard,  
 (iii) involve any Outdoor Storage,  
 (iv) may result in any unusual traffic hazard or congestion,  
 (v) for which any water, air, or other environmental or pollution license or permit is required, or  
 (vi) involve the storage or use of any highly flammable, toxic, explosive, or hazardous matter or materials.

**Heavy Manufacturing:** extraction of natural resources, or transformation of raw materials through mechanical or chemical means into basic products for subsequent assembly, fabrication, or use in the production of finished goods.

**High Albedo Pavement:** a pavement surface that has a cooling effect on the environment, reducing the heat island effect and the energy required to cool Buildings in the area.

**High Albedo Roof:** a light-colored roof that affords high solar reflectance and high emittance and the ability to reflect heat back into the environment. Such roofs prevent heat absorption, which creates a cooling effect that can reduce energy use within the Building.

**Hotel:** an establishment that offers for Transient Occupancy, on a daily or weekly basis for compensation, sleeping accommodations accessible from interior hallways. Not synonymous with Hotel / Motel, Hotel – Full Service, Motel, motor court, tourist court, auto court, motor lodge, or similar facilities. Neither a Hotel nor a room therein is a Short-Term Rental.

**Hotel – Full-Service:** a Hotel that includes on premises a Full-Service Restaurant and one or more of the following additional accommodations: food service, spa, exercise facilities, banquet rooms, meeting or exhibition space.

**House:** See **Table 10-I.4.5.7.1 (Building Types – Summary)** and **Table 10-I.4.5.7.2 (Building Types – Specific Standards)**.

## I

**Impervious Surface:** that portion of a Lot, Building Site, or Development Site, as applicable, covered with Buildings, asphalt, or concrete paving or hard-packed, prepared, graveled surfaces that prevent or restrict absorption of rain or surface water into the ground. If a Lot or Building Site or Development Site, as applicable, is located in more than one (1) District, the Impervious Surface Coverage shall be separately computed for each District and no Impervious Surface Coverage may be transferred between Districts.

**Improvement:** any man-made alteration of land, a Lot or Building Site, or a Building or Structure.

**Industrial Uses:** (1) manufacturing, re-manufacturing, processing, fabricating, creating, producing, packaging, converting, altering, assembling, handling, storing, stockpiling, sorting, recycling, treating, disposing of, wholesaling, warehousing, or distributing materials, products, or information; (2) research and development; (3) trucking, shipping, and transportation; (4) repairing or maintaining commercial or heavy machinery or equipment; and (5) waste management.

**Internal Drive:** a curbed pedestrian and vehicular access way, parallel parking, landscaped area, and public lighting accommodation within a Lot, Building Site, or Development Site that connects to a Thoroughfare or another Internal Drive, and provides access and circulation to or through such Lot, Building Site, or Development Site and the Building(s) and Parking accommodations thereon. An Internal Drive is required to meet the Thoroughfare and Internal Drive assemblies and standards for the applicable District.

## K

**Kennel:** any Structure or premises on which four (4) or more dogs, cats, and/or other household pets over four

## SECTION 10-I.1: DEFINITIONS

(4) months of age are kept for boarding, breeding, raising, selling, grooming, or training, excluding, however, any Structure or premises on which such animals are kept solely for Retail Sale.



**Lamp:** the source of illumination in a lighting fixture.

**Landscape Siting:** locating or maintaining existing evergreen vegetation on the north side of a Building to block winter wind, reducing the need for additional heating; or locating or maintaining existing deciduous vegetation that shades a Building from summer sun, reducing the need for additional cooling.

**Large Commercial:** See **Table 10-I.4.5.7.1 (Building Types – Summary)** and **Table 10-I.4.5.7.2 (Building Types – Specific Standards)**.

**Layer:** a range of depth of a Lot or Building Site within which certain elements are permitted. See **Illustration 10-I.4.3.1 (Layers)**.

The terms First Layer, Second Layer, and Third Layer shall have the following meanings:

**First Layer:** the portion of a Lot or Building Site between the Frontage Line and the line of the Building Facade furthest from the Frontage Line.

**Second Layer:** the portion of a Lot or Building Site between (a) the line of the First Layer which is parallel to and farthest from the Frontage Line and (b) a line which is parallel to and 20 feet from the line described in clause (a). With respect to a Secondary Frontage, the Second Layer and the Third Layer are combined to comprise all parts of the Lot or Building Site which are not within the First Layer.

**Third Layer:** the portion of a Lot or Building Site between (a) the rear lot line and (b) the line of the Second Layer which is parallel to and farthest from

the Frontage Line. With respect to a Secondary Frontage, the Second Layer and the Third Layer are combined to comprise all parts of the Lot or Building Site which are not within the First Layer.

**Legislative Body:** a majority of a regular or special meeting of the Town legally warned, called, and conducted, provided the procedures of Title 30-A Maine Revised Statutes Annotated, Sections 3002 and 4352 are followed.

**Light Industrial:** Industrial Uses characterized by fabrication, creation, processing, production, packaging, converting, altering, assembling, handling, storage, treatment, distribution, disposal, creation, assemblage, processing, or production, and/or treating, designing, fitting, compounding, repairing, assembling, testing, storing, warehousing, wholesaling, distributing, transferring, and/or repair of or from natural, man-made, products or other goods, raw, secondary, partially completed or completed, materials, components, products, by-products, goods, materials, or information, and wholesaling of Industrial, Business, or consumer machinery or equipment, and Light Manufacturing, which Uses are:

- (1) conducted solely within an enclosed Building;
- (2) not detrimental to the environment;
- (3) do not pose any danger or hazard of fire or explosion or other physical hazard;
- (4) do not generate, cause, disseminate, or discharge any pollution, material, dust, smoke, gas, fumes, odor, noise, heat, light, glare, or vibration beyond the premises on which the Building is situated;
- (5) does not involve any Outdoor Storage;
- (6) will not result in any unusual traffic hazard or congestion;
- (7) do not require any water, air, or other environmental or pollution license or permit is required; and
- (8) do not involve storage or use of highly flammable, toxic matter or explosive materials, or of Hazardous Materials.

## SECTION 10-I.1: DEFINITIONS

Not included are Heavy Industrial Uses, manufacture and production of goods from composting organic material; Outdoor Storage, sale or rental of machinery, equipment, heavy trucks, building materials, special trade tools, welding supplies, machine parts, electrical supplies, janitorial supplies, restaurant equipment, or store fixtures and small-scale catering establishments.

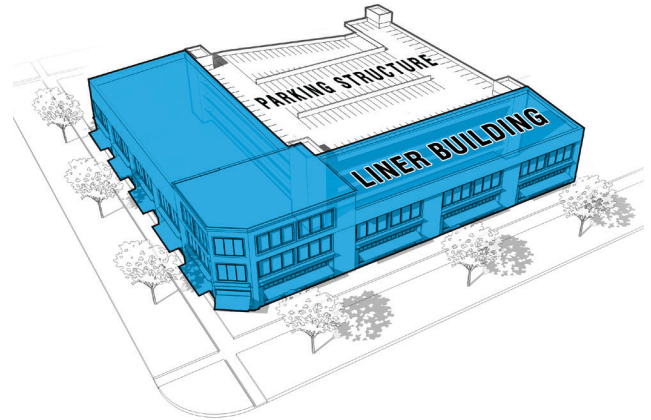
**Light Manufacturing:** the finishing, fabrication, or assembly of previously manufactured parts into a final product or component products ready for retail sale. Light Manufacturing does not having any emissions of noxious odors, dust, fumes, gas, noise, or vibration outside of any Building. See **Table 10-I.4.7.1A (Permitted Principal Uses)**, **Table 10-I.4.7.1B (Permitted Accessory Uses)**, and **Table 10-I.4.7.1C (Permitted Temporary Uses)**.

**Light Shelf:** a horizontal overhang placed in a window above eye level, which reflects daylight onto the ceiling and deeper into a room. The overhang of the shelf also provides shade near the window to reduce window glare.

**Linear Pedestrian Shed:** a Pedestrian Shed that is elongated along an important Corridor. A Linear Pedestrian Shed extends approximately 1/4 mile from each side of the Corridor. The resulting area is oblong. (Syn: elongated pedestrian shed). See **Illustration 10-I.1.12.2 (Linear Pedestrian Shed)**.

**Liner Building:** a Building that is at least 20 feet deep, measured from the Facade, which masks a Parking Lot or a Parking Structure from the Frontage. See **Illustration 10-I.1.L-1 (Liner Building)**.

ILLUSTRATION 10-I.1.L-1 LINER BUILDING



**Live-Work:** See **Table 10-I.4.5.7.1 (Building Types - Summary)** and **Table 10-I.4.5.7.2 (Building Types - Specific Standards)**.

**Lodging:** a general reference to certain Uses within the Commercial Uses category, characterized by overnight accommodations for daily or weekly renting of bedrooms for periods of less than thirty (30) days, including Hotels, Motels, and Lodging Homes / Tourist Homes / Inns.

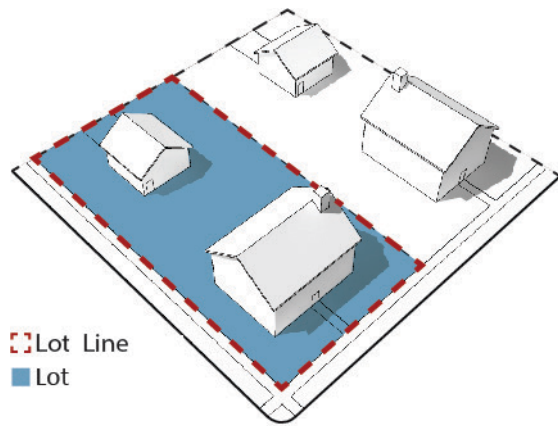
**Lot or Building Site:** a tract, plot, building site, or other parcel of land, or portion thereof, whether or not recorded and whether or not created pursuant to a condominium regime.

**Lot or Building Site Coverage:** the percentage which the total area of surfaces within a Lot or Building Site that is covered by Buildings or other Structures bears to the total gross area of the Lot or Building Site (a ratio of total Impervious Surface area to total Lot or Building Site area).

**Lot or Building Site Line:** the boundary that legally and geometrically demarcates a Lot or Building Site. See **Illustration 10-I.1.L-2 (Lot or Building Site Line)**.

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### ILLUSTRATION 10-I.1.L-2 LOT OR BUILDING SITE LINE



**Lumen:** a measure of brightness.

**Luminaire:** a light unit or fixture including any bulb(s), tube(s), housing, reflective shield, lens and/or ballast.

### M

**Major Subdivision:** any subdivision in which more than four (4) lots are created or more than four (4) dwelling units are created within a five-year period; See also "Subdivision (Major)" in Site Plan and Subdivision Regulations.

**Minor Subdivision:** any subdivision in which not more than four (4) lots are created or in which not more than four (4) dwelling units are created within a five-year period; See also "Subdivision (Minor)" in Site Plan and Subdivision Regulations.

**Mixed Use:** multiple Uses within the same Building or on the same Lot or Building Site through superimposition or Adjacency, or in multiple Buildings or on multiple Lots or Building Sites by Adjacency or proximity.

**Mixed-Use Building:** See **Table 10-I.4.5.7.1 (Building Types - Summary)** and **Table 10-I.4.5.7.2 (Building Types - Specific Standards)**.

**Moderate Income:** as defined in Article 2 of the Town Zoning Ordinance.

**Motel:** an establishment where short-term Lodging in guest rooms is offered for compensation and one (1) or moreroomsareaccessedbyexteriordoorsopeningdirectly to exterior Parking accommodations, passageways, or the outdoors. Not synonymous with Hotel/Motel, Hotel, Hotel - Full Service, Short-Term Rental, Lodging Home / Tourist Home / Inns, Bed and Breakfast.

**Multi-Use Trail:** a Pedestrian Way that accommodates pedestrian and bicycle movement and traverses a Natural Area or rural area, with landscape matching the contiguous Open Space, ideally connecting directly with the urban Sidewalk network. See **Table 10-I.4.11.4 (Pedestrian Way Types)**.

### N

**Natural Area:** a Civic Space type, described as such in **Table 10-I.4.12.2A (Civic Space Types - Summary)** and **Table 10-I.4.12.2B (Civic Space - Specific Standards)**.

**Neighborhood Street:** See **Table 10-I.4.11.2.1B (Thoroughfare & Internal Drive Types -Summary)** and **Table 10-I.4.11.2.1C (Thoroughfare & Internal Drive Assemblies & Standards)**.

**Net Developable Site Area:** the total gross acreage of a site as shown on the applicable proposed or approved proposed Plan, less: 1) the area for Thoroughfares or Internal Drives, 2) other areas of rights-of-way and access, 3) areas designated as Civic Space, and 4) areas which are unsuitable for Development as outlined in Section 7.4 of the Town Site Plan and Subdivision Regulations.

**Site Area:** all land within a site.

### O

**Office Uses:** a Use category characterized by premises available for the transaction of commercial, governmental, professional, medical, dental, not-for-profit, or financial services.



**Operable Windows:** windows which may be opened and closed to provide natural ventilation. Use of operable windows may reduce energy use by approximately 15%. A minimum of 50% of the glazing in a Building should be operable and should be distributed throughout the Building to maximized airflow.

**Outdoor Storage:** one (1) or more assemblages, collections, stacks, or stockpiles of vehicles, equipment, goods, materials, tanks, or other items in any area other than within a Building, including without limitation, parked operative or non-operative vehicles or equipment.

## P

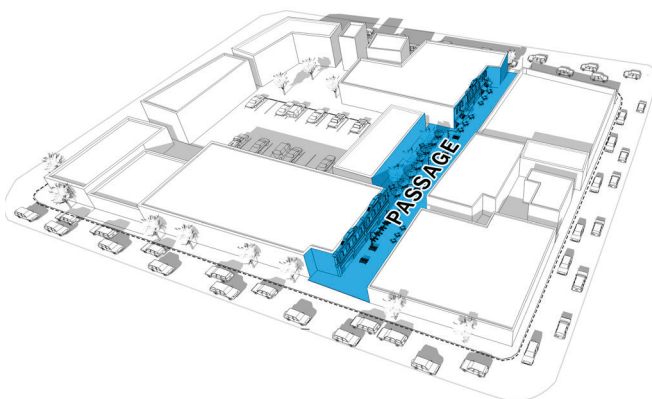
**Parking Area:** an off-street, ground-level open area for Parking vehicles as an Accessory Use incidental to a Principal Use. Not synonymous with Parking Lot.

**Parking Lot:** an off-street, ground-level open area for Parking vehicles as a Principal Use. Not synonymous with Parking Area.

**Parking Structure:** a vertical Improvement containing one or more levels of vehicular Parking above grade.

**Passage:** a Pedestrian Way, open or roofed, which passes between Buildings to provide shortcuts through long Blocks and connect rear Parking accommodations to Frontages. See **Illustration 10-I.1.P-1 (Passage)**. See also **Table 10-I.4.11.4 (Pedestrian Way Types)**.

**ILLUSTRATION 10-I.1.P-1 PASSAGE**



**Path:** a Pedestrian Way traversing a Natural Area or rural area, with landscape matching the contiguous Open Space, ideally connecting directly with the urban Sidewalk network. See **Table 10-I.4.11.4 (Pedestrian Way Types)**.

**Pedestrian Shed:** an area of a size approximately equal to the average distance comfortably walked by pedestrians and approximately centered on a Common Destination. There are two (2) types of Pedestrian Sheds: Standard Pedestrian Sheds and Linear Pedestrian Sheds. See also Standard Pedestrian Shed or Linear Pedestrian Shed. See **Illustration 10-I.2.12.2A (Standard Pedestrian Shed)** and **Illustration 10-I.2.12.2B (Linear Pedestrian Shed)**.

**Pedestrian Way:** an Improvement that accommodates pedestrian movement and may also accommodate bicycles, but which does not accommodate vehicles. Examples of Pedestrian Ways are Sidewalks, Multi-Use Trails, Paths, and Passages. See **Table 10-I.4.11.4 (Pedestrian Way Types)**.

**Photovoltaic (PV):** capable of producing voltage when exposed to radiant energy, especially light.

**Placement:** the manner in which a Building is situated on its Lot or Building Site. See **Illustration 10-I.1.S-2 (Setbacks)**, **Illustration 10-I.4.3.1 (Layers)**, and **Illustration 10-I.4.4.2 (Principal Building / Accessory Building)**. See also "Building Orientation".

**Plan:** any concept plan, sketch plan, site plan, development plan, subdivision plan or other plan submitted pursuant to this Article 10-I or the Town Site Plan and Subdivision Regulations.

**Planter:** the element of the Public Frontage which accommodates Thoroughfare trees, whether continuous or individual, including without limitation, a planting strip.

**Playground:** a Civic Space type, described as such in **Table 10-I.4.12.2A (Civic Space Types - Summary)** and **Table 10-I.4.12.2B (Civic Space - Specific Standards)**.

## SECTION 10-I.1: DEFINITIONS

**Plaza:** a Civic Space type, described as such in **Table 10-I.4.12.2A (Civic Space Types – Summary)** and **Table 10-I.4.12.2B (Civic Space – Specific Standards)**.

**Pocket Park:** a Civic Space type, described as such in **Table 10-I.4.12.2A (Civic Space Types – Summary)** and **Table 10-I.4.12.2B (Civic Space – Specific Standards)**.

**Porch:** an attached exterior covered area large enough to accommodate outdoor furniture and/or gathering; or (2) as a Private Frontage type, one having a Porch and planted Frontage, with the Facade set back from the Frontage Line and used either with or without a Fence to maintain right-of-way spatial definition. See **Table 10-I.4.5.1 (Private Frontage Types)**. A Porch shall be a single level for a one Story Building. If the Building is two (2) Stories, a porch may be either one (1) or two (2) levels, with the upper level being roofed or uncovered.

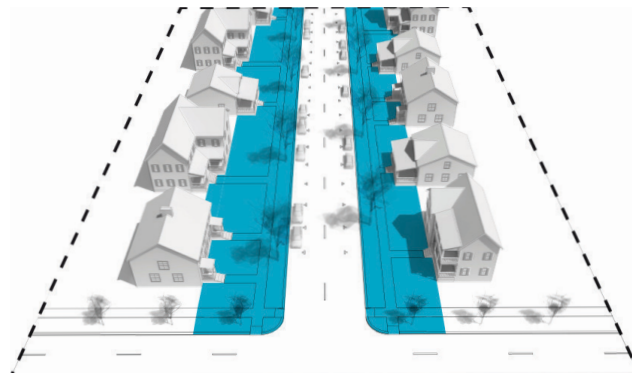
**Principal Building:** as defined in Article 2 of this Ordinance, except that the phrase “any Principal Use” shall be substituted for the phrase “the principal use”.

**Principal Entrance:** the main point of access for pedestrians into a Building.

**Principal Frontage:** (a) with respect to Corner Lots and Building Sites and Through Lots and Building Sites, the Private Frontage designated to bear the address and the Principal Entrance to the Building, and the measure of Lot or Building Site Width. For Corner Lots and Building Sites, prescriptions for the location of Parking in certain Layers pertain only to the Principal Frontage and prescriptions for the First Layer pertain to both Frontages of a Corner Lot or Building Site; and for Through Lots or Building Sites, prescriptions for the location of Parking in certain Layers pertain to the Principal Frontage and the opposite Frontage and prescriptions for the First Layer pertain to all Frontages; (b) with respect to Lots or Building Sites that are neither Corner Lots or Building Sites, nor Through Lots or Building Sites, Principal Frontage is synonymous with Frontage. See Frontage, **Illustration 10-I.4.5.2 (Frontage**

**& Lot Lines)** and **Illustration 10-I.1.P-2 (Principal Frontage)**.

### ILLUSTRATION 10-I.1.P-2 PRINCIPAL FRONTAGE



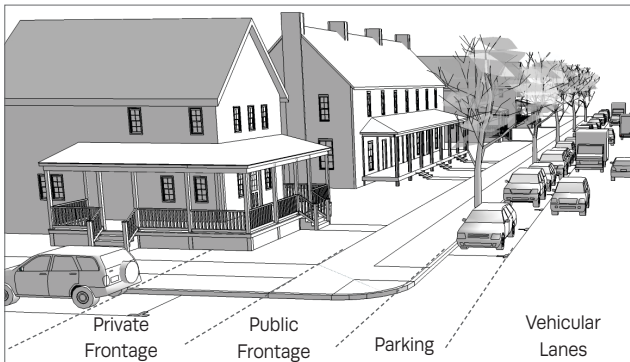
■ Principal Frontage

**Principal Use:** as defined in Article 2 of this Ordinance, except that the phrase “any Principal Use” shall be substituted for the phrase “the principal use”.

**Private Frontage:** the privately held area between the Frontage Line and the Principal Building Facade. See **Illustration 10-I.4.5.2 (Frontage & Lot or Building Site Lines)**, **Table 10-I.4.5.1 (Private Frontage Types)**, and **Illustration 10-I.1.P-3 (Private/Public Frontage)**.

**Public Frontage:** the area between the Frontage Line and (a) in the case of a Lot or Building Site that Enfronts a Thoroughfare, the Curb (or if there is no Curb, the edge) of the Vehicular Lanes, or (b) in the case of a Lot or Building Site that Enfronts a Civic Space or Sidewalk, such Civic Space or Sidewalk. See **Illustration 10-I.4.5.2 (Frontage & Lot or Building Site Lines)** and **Illustration 10-I.1.P-3 (Private/Public Frontage)**. See also **Table 10-I.4.11.2.1B (Thoroughfare & Internal Drive Types – Summary)** and **Table 10-I.4.11.2.1C (Thoroughfare & Internal Drive Assemblies & Standards)**.

**ILLUSTRATION 10-I.1.P-3 PRIVATE/PUBLIC FRONTAGE**



**Public Transportation Station:** a Building or Structure at which passenger loading or unloading of any mode of public transportation is accommodated. For purposes of this Article 10-1, Public Transportation Station is included within the Civic & Public Use category and is not included within the Vehicle Use category.

## R

**Rearyard:** the yard type resulting from a Building occupying the full Frontage Line, leaving the rear of the Lot or Building Site as the sole yard.

**Regulating Plan:** a map or set of maps that shows the Districts, Civic Zones, Thoroughfares, Internal Drives, and Special Requirements if any, of areas subject to, or proposed to be subject to, regulation by this Article 10-I.

**Residential:** Use category pertaining to premises available for long-term, non-Transient human habitation.

**Restaurant:** an establishment, which is neither a Formula Restaurant nor a Fast-Food Restaurant, in a site-built Building whose primary business is the preparation, serving, and sale of meals on the premises for consumption on or off of the premises, regardless of whether or not alcoholic beverages are served. Brew Pubs and Taverns are Restaurant types provided that they meet the primary business element of this definition. Bars are

not Restaurants. See definitions of Formula Restaurant and Fast Food Restaurant in Article 2 of Town Zoning Ordinance, and Restaurant – Full Service, Fast Food Restaurant, Brew Pub, Tavern, and Bar or Tavern in this Section 10-I.1.

**Restaurant – Full Service:** a Restaurant without drive-through facilities, which offers complete and varied menus for breakfast, lunch, or dinner, provides table service to guests and others for such meals, and employs a server staff to take orders and deliver food. The Restaurant may have ancillary take-out service.

**Retail Sales:** offering, selling, renting, or leasing new or used goods in person, on-line, or by mail in transactions dealing directly with the consumer of such goods.

**Road:** See **Table 10-I.4.11.2.1B (Thoroughfare & Internal Drive Types – Summary)** and **Table 10-I.4.11.2.1C (Thoroughfare & Internal Drive Assemblies & Standards)**.

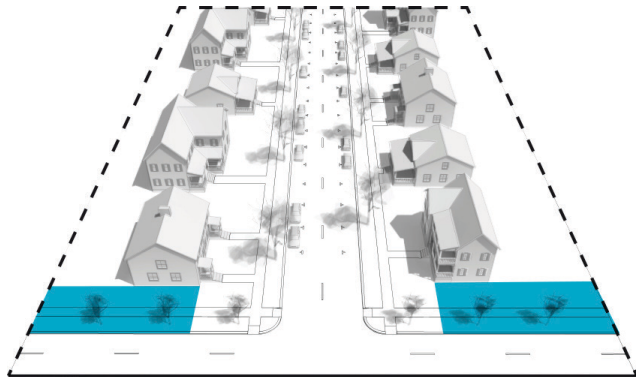
## S

**Screen:** (a) as a verb, to block an item or condition from view from a vantage point in accordance with the requirements of this Article 10-I by interposition of (i) a Building, Building Element, wall or Fence constructed of an opaque material, or (ii) an evergreen hedge; (b) as a noun, a Building, Building Element, wall, Fence or evergreen hedge which blocks an item or condition from view from a vantage point. Not synonymous with Streetscreen.

**Secondary Frontage:** on Corner Lots or Building Sites, the Private Frontage that is not the Principal Frontage. See **Illustration 10-I.1.S-1 (Secondary Frontage)**.

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ILLUSTRATION 10-I.1.S-1 SECONDARY FRONTAGE



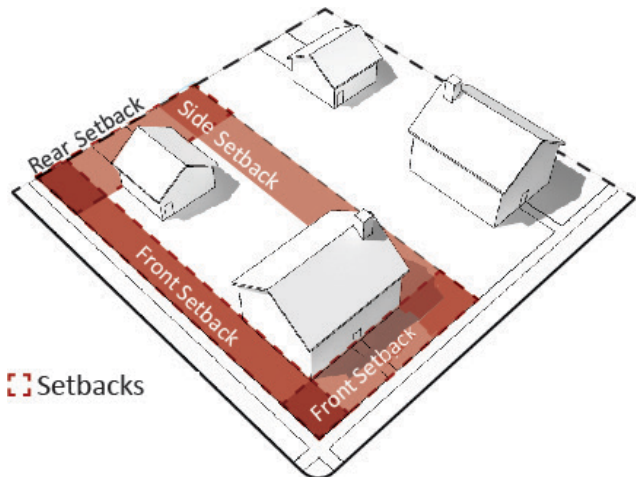
■ Secondary Frontage

**Service Business:** offering or providing a service to or for a customer for compensation, in which the provider of such service deals directly with the consumer. A Service Business may include the Retail sale of goods related to the services provided.

Excluded from Services, for purposes of this Article, are services provided by a medical, dental or veterinary professional, vehicle-related services, and Obscene Exhibitions.

**Setback:** the area of a Lot or Building Site measured from the Lot or Building Site Line to a Building Facade, which is maintained clear of permanent Structures, with the exception of permitted Encroachments, as indicated in **Illustration 10-I.1.S-2 (Setbacks)** and **Illustration 10-I.4.4.1 (Setback Designations)**.

ILLUSTRATION 10-I.1.S-2 SETBACKS



■ Setbacks

**Shading of Glazing:** the practice of blocking sunlight from

**YORK BEACH GREENWAY DISTRICT  
ZONING AMENDMENT**

entering a window to prevent excess heat gain in warm months. See **Table 10-I.4.14.1C (Shading of Glazing)**.

**Shopfront:** a Private Frontage type conventional for Commercial Use, with substantial glazing, wherein the Facade is aligned close to the Frontage Line with the Building entrance at Sidewalk grade. See **Table 10-I.4.5.1 (Private Frontage Types)**.

**Short-Term Rental:** a Residential property that is rented for vacation, leisure or recreation purposes for a week or a month, and typically under thirty (30) days but not for more than an entire summer or winter season, to a Person who has a place of permanent residence to which the Person intends to return.

Not synonymous with Hotel / Motel, Hotel, Hotel – Full Service, Motel, Bed and Breakfast, or Lodging Home / Tourist Home / Inn.

**Sidewalk:** the paved section of the Public Frontage dedicated to pedestrian activity, and a type of Pedestrian Way. See **Table 10-I.4.11.4 (Pedestrian Way Types)**.

**Sideyard:** the yard type resulting from a Building occupying one side of its Lot or Building Site with a Setback on the other side.

**Sideyard Building:** a Building that occupies one side of its Lot or Building Site with a Setback on the other side.

**Small Multi-Family Building:** See **Table 10-I.4.5.7.1 (Building Types – Summary)** and **Table 10-I.4.5.7.2 (Building Types – Specific Standards)**.

**Solar Reflective Index (SRI):** a measure of a constructed surface's ability to reflect solar heat, as shown by a small temperature rise. Numerically, standard black is 0 and a standard white is 100.

**Special Exception:** a Use that is permitted pursuant to review and approval by the Town Board of Appeals in accordance with Section 18.8.2.2 of the Town Zoning Ordinance.



## SECTION 10-I.1: DEFINITIONS

**Special District:** an area on a Regulating Plan to which certain Development, Building, and Lot or Building Site standards are applied, for which, due to the necessary and intrinsic uses, size, or form of the proposed Development within such area, such Development cannot under any plan, design, or circumstances conform to one or more of the Character Districts and the applicable standards therefor.

**Special Requirements:** as described in Section 10-I.2.6.3 and/or the associated designations on a Plan for those provisions.

**Special Permit:** a permit issued pursuant to review and approval by the Town Code Enforcement Officer and/or the Planning Board, as applicable, with respect to certain activities with designated shoreland or wetland areas in accordance with Article 18 of the Town Zoning Ordinance.

**Sport Field or Court:** a Civic Space type, described as such in **Table 10-I.4.12.2A (Civic Space Types – Summary)** and **Table 10-I.4.12.2B (Civic Space – Specific Standards)**.

**Square:** a Civic Space type designed for unstructured recreation and Civic purposes, spatially defined by Building Frontages and consisting of Paths, lawns and trees, formally disposed. See **Table 10-I.4.12.3A (Civic Space Types – Summary)** and **Table 10-I.4.12.3B (Civic Space Types – (Specific Standards))**.

**Standard Pedestrian Shed:** a Pedestrian Shed that is an average 1/4 mile radius or 1,320 feet, about the distance of a five-minute walk at a leisurely pace. See **Pedestrian Shed** and **Illustration 10-I.2.12.2A (Standard Pedestrian Shed)**.

**Stepfront:** a Private Frontage wherein the Facade is aligned close to the Frontage Line with an entrance that usually has a single step without a landing. See **Table 10-I.4.5.1 (Private Frontage Types)**.

**Stoop/Stoopfront:** a Private Frontage type wherein the Facade is aligned close to the Frontage Line with the

first Story elevated from the Sidewalk for privacy, with an exterior stair and landing at the entrance. See **Table 10-I.4.5.1 (Private Frontage Types)**.

**Streetscreen:** a type of Screen situated along a Frontage Line, or along the same plane as a Frontage Facade, and consisting of a planted evergreen hedge or freestanding wall constructed of brick or stucco over masonry, which blocks the view of Parking, equipment, or other item or condition from the Frontage, provides privacy to a side yard, or strengthens the spatial definition of the public realm.

**Story:** a habitable level within a Building, excluding an Attic or raised Basement.

**Structural Soil:** a load-bearing soil that resists compaction to allow for healthier tree root growth. Angular gravel within the soil mix allows air and water to permeate while supporting pavement loads.

**Surface to Volume Ratio (S/V Ratio):** the amount of surface exposed to the outside per volume of Building unit. See **Table 10-I.4.14.1B (Surface-to-Volume Ratio & Building Orientation)**.

**Sustainability:** the basis upon which an organism or a community can manage its own continuing viability, meeting the needs of the present without compromising the ability of future generations to meet their own needs.

### T

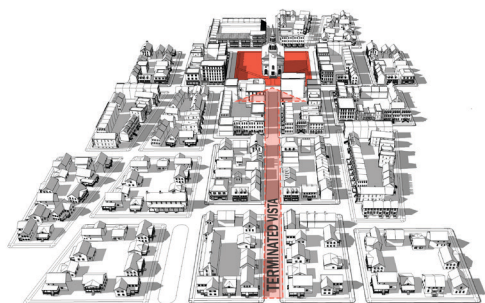
**Tavern:** an establishment which meets the primary business element of the definition of Restaurant and which may serve alcoholic beverages for consumption on site.

**Terminated Vista:** a location at the axial conclusion of a Thoroughfare. A Building located at a Terminated Vista location designated on a Regulating Plan is required to be designed in response to the axis. See **Illustration 10-I.1.T-1 (Terminated Vista)**.



## SECTION 10-I.1: DEFINITIONS

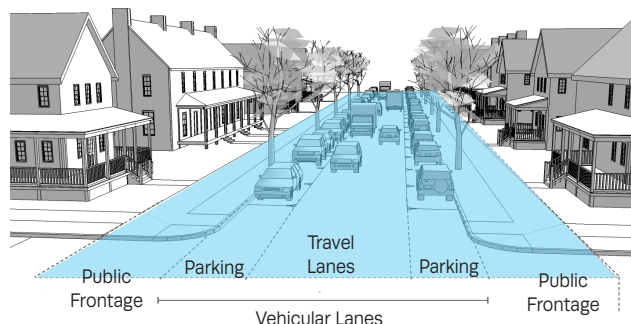
ILLUSTRATION 10-I.1.T-1 TERMINATED VISTA



**Terrace/Lightwell:** a Private Frontage type wherein the Facade is set back from the Frontage Line by an elevated landing and sunken Lightwell. This type buffers Residential Uses from urban Sidewalks with the first Story elevated from the Sidewalk for privacy, and may allow for activation of space below grade. An exterior stair and landing provides access to the entrance. This type is recommended for ground-floor Residential Use. See **Table 10-I.4.5.1 (Private Frontage Types)**.

**Thoroughfare:** a way for use by vehicular and pedestrian traffic and to provide access to Lots, Building Sites and Open Spaces, consisting of Vehicular Lanes and the Public Frontage. See **Illustration 10-I.1.T-2 (Thoroughfare)**, provided that neither a shared drive nor an Alley is a Thoroughfare.

ILLUSTRATION 10-I.1.T-2 THOROUGHFARE



**Through Lot or Building Site:** a Lot or Building Site that abuts two (2) essentially parallel streets, which is not a Corner Lot or Building Site.

**Tiny Home:** a dwelling that meets the definition of Tiny Home as defined in 29-A MRS §101(80-C) and also meets the description of the Building Type designated as such in **Table 10-I.4.5.7.1 (Building Types – Summary)** and **Table 10-I.4.5.7.2 (Building Types – Specific Standards)**.

**Townhouse:** See **Table 10-I.4.5.7.1 (Building Types – Summary)** and **Table 10-I.4.5.7.2 (Building Types – Specific Standards)**.

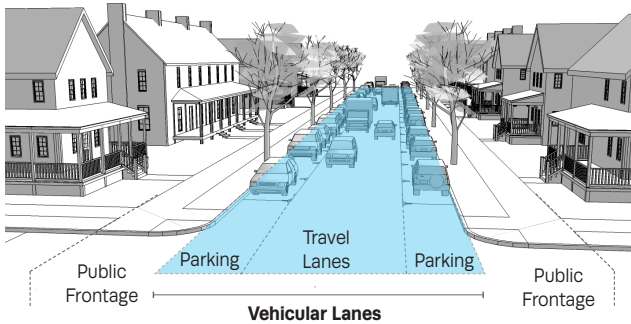
### U

**Use:** the functions, activities or uses accommodated by a Building, Structure, Lot or Building Site. See **Table 10-I.4.7.1A (Permitted Principal Uses)**, **Table 10-I.4.7.1B (Permitted Accessory Uses)**, and **Table 10-I.4.7.1C (Permitted Temporary Uses)**.

### V

**Vehicular Lane:** an area of a Thoroughfare for parked or moving vehicles. See **Illustration 10-I.1.V (Vehicular Lane)**.

ILLUSTRATION 10-I.1.V VEHICULAR LANES



## W

**Workforce Affordable Housing:** a single-family, two-family, or multi-family Dwelling Unit, or any combination of these, which may be purchased or rented for year-round occupancy by a working household with Moderate Income.

## SECTION 10-I.1: DEFINITIONS

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# SECTION 10–I.2

## GENERAL

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### SECTION 10–I.2.1 COMMON REFERENCE.

This Article 10–I of the Town Zoning Ordinance, as the same may be amended from time to time, is referred to herein, and otherwise may be referred to as this “Article” or this “Article 10–I”.

### SECTION 10–I.2.2 INTENT.

The intent of this Article 10–I is to provide for walkable, mixed-use, and character-based Buildings, Lots, Building Sites, Development, and re-Development within all or such parts of the Town as are designated as Character-Based Districts or Special Districts pursuant to this Article 10–I, as identified from time to time on the Town Zoning Map.

Approval of a Regulating Plan must be obtained as a zoning map amendment (each, a “Regulating Plan”, or if more than one, “Regulating Plans”) in order for an area to be governed by this Article 10–I. See Sections 10–I.2.6 and 10–I.2.7.

### SECTION 10–I.2.3 APPLICABILITY.

#### 10–I.2.3.1 General.

This Article 10–I, each Regulating Plan, and the standards and requirements hereof, shall be applicable to all areas and to all Development, re-Development, Improvements, land, Structures, modifications, construction, re-construction, Buildings, Lots, and Building Sites in such areas upon approval of a Regulating Plan for such areas.

### 10–I.2.3.2 Relationship to Other Provisions.

1. The provisions of this Article 10–I shall take precedence over those of other Town codes, ordinances, regulations, and standards that may be in conflict with this Article 10–I, except Town health and safety codes and, to the extent applicable to certain portions of the Town to which this Article 10–I are or may become applicable, the following provisions of the Town Zoning Ordinance:

- a. The following sections of Article 3 (Establishment of Zoning Districts) –
  - (1) Section 3.8 (Shoreland Overlay District),
  - (2) Section 3.9 (Watershed Protection Overlay District),
  - (3) Section 3.12 (Wetlands Protection Overlay District), and
  - (4) Section 3.16 (Workforce Affordable Housing Overlay District);
- b. Article 6 (Supplemental Use Requirements);
- c. Article 8 (Shoreland Overlay District);
- d. Article 10 (Watershed Protection Overlay District);
- e. Article 10–F (Workforce Affordable Housing Overlay District);
- f. Article 11 (Wetland Protection Overlay District);
- g. Article 12 (Historic & Archeological Resources);

## SECTION 10-I.2: GENERAL

h. Article 17 (Non-Conforming Situations);

i. Article 18 (Administration); and

j. Article 18-A (Application Review Procedures).

2. Without limitation to Section 10-I.2.3.2.1, the provisions of this Article 10-I shall take precedence over any of the following that may be in conflict with this Article 10-I:

a. any other provision of the Town Zoning Ordinance; and

b. any provision of the Site Plan and Subdivision Regulations of the Planning Board of the Town of York.

3. Subject to Sections 10-I.2.3.2.1 and Section 10-I.2.3.2.2, all Town codes, ordinances, regulations, and standards (collectively, the "Existing Local Codes"), including without limitation, the Site Plan and Subdivision Regulations of the Planning Board of the Town of York, and all other provisions of the Town Zoning Ordinance, shall continue to be applicable to matters not covered by this Article 10-I.

4. This Article 10-I supersedes in its entirety with respect to any area covered hereby the York Beach Village Design Standards enacted November 11, 2010.

### 10-I.2.3.3 Exclusive Zoning Regulation.

Except as may be otherwise specifically provided in this Article 10-I, including without limitation Section 10-2.1.3.2, upon approval of a Regulating Plan for an area, this Article 10-I shall be the exclusive and mandatory zoning regulation for such area, and the provisions of this Article 10-I shall be applied to all such area.

### 10-I.2.3.4 Compliance.

Except for legal non-conformities allowed under Article 17 of the Town Zoning Ordinance, within an area for which a Regulating Plan has been approved, all Development, re-Development, land, Improvements, construction,

re-construction, modifications, Structures, Buildings, Lots, and Building Sites, and all Plans, applications, and submissions required or submitted under this Article 10-I, must comply with this Article 10-I and the standards hereof as the same are in effect at the time of submission of the complete application for a proposal, and pursuant to the applicable approved Regulating Plan or other any applicable approved Plan.

## SECTION 10-I.2.4

### TABLES, MAPS, PHOTOGRAPHS, IMAGES, ILLUSTRATIONS, FIGURES, GRAPHICS, DIAGRAMS & DEPICTIONS.

1. Maps and the standards and requirements of Tables herein are an integral part of this Article 10-I.

2. The diagrams, photographs and illustrations in **Table 10-I.4.2.1 (District & Civic Space Standards)**, **Table 10-I.4.5.1 (Private Frontage Types)**, **Table 10-I.4.5.7.1 (Building Types - Summary)**, **Table 10-I.4.5.7.2 (Building Types - Specific Standards)**, **Table 10-I.4.12.2A (Civic Space Types - Summary)** and **Table 10-I.4.12.2B (Civic Space Types - Specific Standards)** are provided only to indicate the general character or placement of and/or reference to the various Districts and elements thereof, shown thereon, and they shall have regulatory force and effect only to that extent.

3. The illustrations in **Table 10-I.4.10.1-A (Private Lighting Types)** and **Table 10-I.4.10.1-B (Public Lighting Types)** are provided only as an approximation of the various lighting types, and they shall have regulatory force and effect only to that extent.

4. All graphical and tabular depictions entitled "Illustration" or "Figure", or denoted as "illustrative" are provided for illustrative, explanatory purposes only, and are not regulatory.

5. Where in conflict, numerical metrics shall take precedence over graphic metrics.



**SECTION 10-I.2.5  
DEFINITIONS.**

Capitalized terms used throughout this Article 10-I may be defined in Section 10-I.1 (Definitions) and may be defined elsewhere in this Article 10-I and other parts of the Town Zoning Ordinance. In the event of any conflict between the definitions in this Article 10-I and other definitions of the Town Zoning Ordinance, those of this Article 10-I shall take precedence. The meaning of terms not so defined shall be accorded the meaning given to such terms as applicable in to the context in which such terms are used.

**SECTION 10-I.2.6  
PREPARATION AND ADOPTION OF  
REGULATING PLANS.**

**10-I.2.6.1 Initial Regulating Plan  
and Amendments.**

An initial Regulating Plan for each area to be regulated by this Article 10-I shall be prepared by or on behalf of the Town, and shall be submitted, reviewed, and acted upon as required for a zoning map amendment. This Article 10-I shall be effective with respect to such areas of the Town when and for which a Regulating Plan has been adopted for such areas.

Any additional Regulating Plans and amendments to Regulating Plans must be prepared, submitted and acted upon as zoning map amendments.

**10-I.2.6.2 Contents of Regulating  
Plan.**

In addition to all other items required for a zoning map amendment, each Regulating Plan and each amendment thereto shall reflect for the area to be regulated thereby:

1. existing zoning districts and proposed districts;
2. proposed and existing Thoroughfares and Internal Drives, existing Thoroughfares and Internal Drives, and any proposed changes to existing Thoroughfares and Internal Drives; and

3. any existing and proposed applicable Special Requirements, as described in Section 10-I.6.3 below.

Unless prepared by or on behalf of the Town, each Regulating Plan and each Regulating Plan amendment shall include a concept plan that generally describes and shows any Development, re-Development, land, construction, re-construction, modifications, Structures, Improvements, Buildings, Lots, and Building Sites contemplated or to be proposed.

**10-I.2.6.3 Special Requirements.**

A Regulating Plan may designate any of the Special Requirements listed below. If a Regulating Plan designates any one or more of such Special Requirements, such designation indicates that the following standards shall be applied as follows:

1. A Shopfront Frontage Special Requirement designation, which requires that each Private Frontage within the designated area be provided a Shopfront Frontage at Sidewalk level, except at any allowed Driveways or Streetscreen areas, as provided in **Table 10-I.4.5.1 (Private Frontage Types)** and specified in this Article 10-I.
2. A Terminated Vista Special Requirement designation, which requires that a Building be provided with a cupola, chimney, entry feature or habitable tower that intersects the centerline axis of the view to which they respond.
3. A Cross Block Passage Special Requirement designation, which requires that a minimum 10-foot-wide pedestrian access be reserved between Buildings.
4. A Special Height Range Special Requirement designation, which requires that Buildings within the designated area comply with the Building Height noted rather than the Building Height standard that otherwise would be applicable within the District.
5. A Residential Use Restriction Special Requirement designation, which prohibits Uses categorized as

## SECTION 10-I.2: GENERAL

Residential in **Table 10-I.4.7.1A (Permitted Principal Uses)** within the ground floor of Buildings within the designated area, irrespective of whether Residential Use would otherwise be permitted within the applicable District.

6. A Residential Use Only Special Requirement designation, which requires that the only Principal Uses that are allowed within the designated area are Uses categorized as Residential in **Table 10-I.4.7.1A (Permitted Principal Uses)**, irrespective of whether other Principal Uses would otherwise be permitted within the applicable District.

7. A Retail Use Only Special Requirement designation, which requires that each Private Frontage within the designated area be provided a Shopfront Frontage at Sidewalk level, except at any allowed Driveways or Streetscreen areas, as provided in **Table 10-I.4.5.1 (Private Frontage Types)** and that the ground level be available only for Retail Use.

8. A Route 1 Special Requirement designation, which provides for the designated area a maximum Lot or Building Site Width of 150 feet, and Flex Buildings and Large Commercial Buildings as two additional permitted Building Types.

9. A Neighborhood Corner Retail/Service Use Special Requirement designation, which indicates that Retail and Service Principal Uses are permitted on the Lot or Building Site as provided in **Table 10-I.4.7.1A (Permitted Principal Uses)** with any of the permitted Private Frontage types and Building Types for the applicable District, or if not otherwise permitted in the applicable District, with a Shopfront Frontage in a Live/Work Building.

10. A Light Industrial Use Special Requirement designation, which indicates that Light Industrial Uses are permitted on a Lot or Building Site that legally existed immediately prior to the enactment of this Article as provided in **Table 10-I.4.7.1A (Permitted Principal Uses)** if such Lot or Building Site abuts

land that has a Lot or Building Site that has a Route 1 Special Requirement designation as described in Section 10-I.2.6.3.8 above and are located east of Short Sands Road.

11. A Corner Parking Lot (Commercial) Special Requirement designation, which indicates that a Parking Lot (Commercial) is permitted by Special Use Permit on a Lot or Building Site that legally existed immediately prior to the enactment of this Article as provided in **Table 10-I.4.7.1A (Permitted Principal Uses)** if Screened from view from all Frontages and abutting Lots and Building Sites, as indicated in **Table 10-I.4.2.1 (District & Civic Space Standards: CD-3 Neighborhood Character District)**.

12. A Main Street Height Special Requirement designation, which indicates that any Structure Enfronting Main Street shall not exceed thirty-five (35) feet in Height.

### SECTION 10-I.2.7 REGULATING PLAN AMENDMENT.

Each Regulating Plan in effect from time to time, may be amended by a zoning map amendment upon initiative of the Town or upon application of the Owner of the parcel for which a zoning change is proposed to the Planning Department/Planning Board for zoning amendment consideration.

### SECTION 10-I.2.8 DETERMINATION OF STANDARDS.

The Legislative Body has established the standards in this Article 10-I and, upon approval of a Regulating Plan for an area, will have made such standards applicable to such area.

**SECTION 10-I.2.9  
APPLICATIONS.**

**10-I.2.9.1 Regulating Plans & Amendments.**

Applications for approval of a proposed Regulating Plan, a Regulating Plan amendment, or any other Plan that would require or proposes any change in the District designation of any property subject to this Article 10-I shall necessitate processing, review, consideration, and/or action with respect to such change in designation as a zoning map amendment under the Town Zoning Ordinance.

**10-I.2.9.2 Site Plans and Subdivision Plans.**

No Development, re-Development, Improvement, subdivision, re-subdivision, construction, re-construction, or modification of or on any Building, Structure, Lot, or Building Site subject to this Article 10-I shall occur in the absence of an approved Plan evidencing compliance this Article 10-I.

Any Plan required or submitted under the Town Zoning Ordinance or the Town Site Plan and Subdivision Regulations shall include, in addition to any other required items and information, the applicable items listed in Section 10-I.2.12.2.

In addition to satisfaction of all other applicable Plan or permit applications, approval, or issuance requirements, no such Plan, application, or permit may be approved or issued in the absence of a determination by each applicable Decision-Making Authority that the same complies with all applicable standards and requirements of this Article 10-I and the applicable Regulating Plan.

**SECTION 10-I.2.10  
PLANS.**

**10-I.2.10.1 Plan Required.**

Except as otherwise provided under Article 17 (Non-Conforming Situations) for legal non-conformities, no

Building shall be constructed, re-constructed, modified or exist, no Lot or Building Site shall be Developed, Improved, subdivided, re-subdivided or exist, and no Plan or permit shall be approved or issued except in compliance with this Article 10-I and the applicable approved Regulating Plan and pursuant to a Plan for such Building, Lot or Building Site, and Development, as applicable, which has been prepared, submitted, and approved in accordance with this Article 10-I and all standards and requirements applicable thereto.

**10-I.2.10.2 Preparation and Requirements.**

Each Regulating Plan, Plan, application, or any amendment thereof, for one or more proposed Buildings, Lots or Building Sites, and/or Development, as applicable, shall be prepared in accordance with, and shall conform to the requirements of this Section 10-I.1 and the other applicable provisions of this Article 10-I.

**10-I.2.10.3 Submission.**

Each Regulating Plan, Plan, application, or any amendment thereof, for one or more proposed Buildings, Lots or Building Sites, and/or Development, as applicable, shall be submitted for review and action in accordance with this Article 10-I.

**10-I.2.10.4 Approval / Issuance.**

Action shall be taken on an application for approval or issuance of a Regulating Plan, Plan, or permit, or any amendment thereof, for one or more proposed Buildings, Lots or Building Sites, , and/or Development, as applicable, in accordance with this Section 10-I.2 and other applicable provisions of this Article 10-I.

**SECTION 10-I.2.11  
PREPARATION & SUBMISSION BY  
OR FOR OWNER.**

Each Regulating Plan, Plan, or application for approval or issuance of a permit for one or more proposed Buildings, Lots or Building Sites, and/or Development, as applicable, required under this Article 10-I shall be prepared and

## SECTION 10-I.2: GENERAL

submitted by or on behalf of the Owner of the applicable property.

### SECTION 10-I.2.12 PLAN & APPLICATION REQUIREMENTS.

Each Regulating Plan, Plan, or application for approval or issuance of a permit submitted for any Building, Lot or Building Site, and/or Development located or proposed within an area regulated under this Article 10-I shall:

#### 10-I.2.12.1 Compliance.

Comply with the applicable approved Regulating Plan for such Building, Lot or Building Site, and/or Development and all standards and requirements of this Article 10-I applicable to the District in which the land covered by such application for approval or issuance of a permit is situated; and

#### 10-I.2.12.2 Contents.

Show and include, in addition to all other items required by the Town Zoning Ordinance or Town Site Plan and Subdivision Regulations, as applicable, the following, in compliance with the standards and requirements of any applicable approved Plan, the applicable Regulating Plan, and this Article 10-I for the proposed Buildings, Lot or Building Sites, and/or Development, as applicable:

1. Lot or Building Site Width
2. Lot or Building Site Coverage
3. Lot or Building Site Layers
4. Frontage Buildout
5. Principal Buildings
6. Accessory Buildings, including the habitable area of each
7. Setbacks
8. Encroachments

9. Building Types
10. Building Heights
11. Private Frontages
12. Facade and each other Building Elevation
13. Building Roof Type & Pitch
14. Principal Uses, and Accessory Uses
15. Private Landscaping, including Walls and Fences
16. Private Lighting
17. Existing and proposed Pedestrian Ways
18. Vehicular Parking Accommodations, including Parking Structures, Parking Areas, Parking Lots, number of Parking spaces within the Lot, Parking Location, Location of any Garage, Driveway to off-street Parking, Passenger Drop-off Location, Driveway/Vehicular Entrance Width, any Parking Structure exit locations, and any Parking screening
19. Alternative forms of transportation
20. An Open Space Plan and a Pedestrian Way Plan, which shall be submitted subject to approval during the Conceptual Design Review Process
21. Loading, Service, Storage, Trash Receptacle, Utility Box, Service Meter, and other equipment accommodations and locations
22. Personal property or fixtures not affixed to a Building, if any
23. Screens and Streetscreens
24. Signage
25. Any applicable Special Requirements
26. For any proposed new Building that does not have public sewer, a plan acceptable to the Town showing all proposed primary and secondary septic fields and a percolation test showing all results and test locations.



**27.** For any new Building that does not have public water service, an easement protected well plan acceptable to the Town of York

**28.** For any Major Subdivision, proof of availability of public water and public sewer service

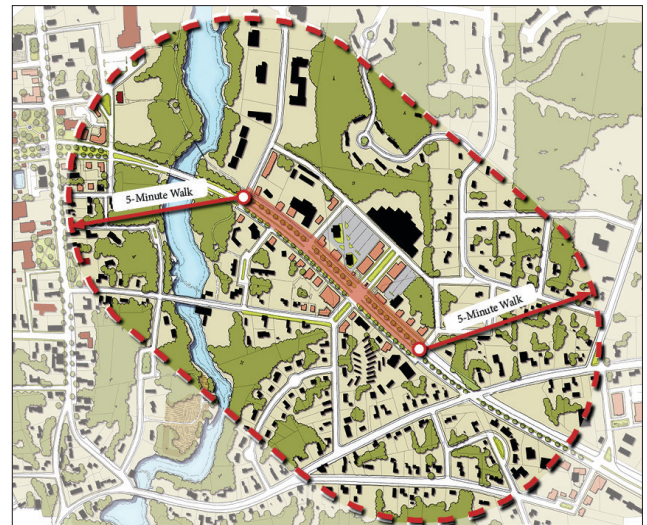
**29.** For any Development Site of five (5) or more Net Developable Acres:

- a. any required or proposed Civic Spaces or Civic Buildings;
- b. existing zoning districts, Character Districts, or Special Districts and any changes proposed or to be proposed, assigned according to and as required by Section 10-I.3.2 and Section 10-I.4;
- c. Thoroughfares and Internal Drives, and each element thereof;
- d. Block Perimeter;
- e. Public Lighting outside of Public Frontages; and
- f. one or several proposed or existing Standard or Linear Pedestrian Sheds, as applicable, located according to existing or proposed conditions, such as traffic intersections, Adjacent Development, natural features, and their respective Common Destinations. See **Illustration 10-I.2.12.2A (Standard Pedestrian Shed)** and **Illustration 10-I.2.12.2B (Linear Pedestrian Shed)**, following:

**ILLUSTRATION 10-I.2.12.2A STANDARD PEDESTRIAN SHED**



**ILLUSTRATION 10-I.2.12.2B LINEAR PEDESTRIAN SHED**





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# SECTION 10–I.3

## DISTRICTS & CIVIC SPACES

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### SECTION 10–I.3.1 GENERAL.

#### 10–I.3.1.1 Applicability and Compliance.

This Section 10–I.3 applies to Character Districts, Special Districts, and Civic Spaces to the extent provided herein.

Development, re-Development, land, Improvements, construction, re-construction, modification, subdivision, re-subdivision, Structures, Buildings, Lots, and Building Sites within each District or Civic Space, as applicable, shall include the respective elements and shall comply with the respective standards applicable to each.

#### 10–I.3.1.2 Districts and Civic Space Defined.

Character District, Special District, District, and Civic Space are defined in Section 10–I.1.

#### 10–I.3.1.3 District Descriptions.

This Article 10–I provides for the following Districts, each of which is described generally in **Table 10–I.4.2.1 (District & Civic Space Standards)**:

1. Character District CD–1 (Natural);
2. Character District CD–3 (Neighborhood);
3. Character District CD–4 (Neighborhood Center); and

4. Special District SD–AE (Amusement & Entertainment).

### SECTION 10–I.3.2 ASSIGNMENT OF DISTRICTS & CIVIC SPACE.

#### 10–I.3.2.1 General.

Districts and Civic Spaces shall be assigned and mapped on a Regulating Plan and any change to such assignment requires a zoning map amendment.

#### 10–I.3.2.2 Assignment of Civic Space for Certain Development Sites.

Except for assignment of Civic Space on Regulating Plans prepared by or on behalf of the Town, each Development Site of five (5) or more acres of Net Developable Site Area shall include at least 5%, but not more than 20%, of its Net Developable Site Area assigned as Civic Space(s).

#### 10–I.3.2.3 Special Districts.

1. For any Development Site of five (5) or more acres of Net Developable Site Area, excluding any Special Districts, an application for approval of a proposed Regulating Plan or Zoning Map Amendment or any proposed Development not initiated and prepared by the Town, may request approval of a Special District only if, due to the necessary and intrinsic uses, size, or form, the proposed Development cannot under any plan, design, or circumstances conform to one

(1) or more of Districts CD-1, CD-3, or CD-4 and the applicable standards therefor. A request for approval of a Special District must contain proposed standards to be applicable within the Special District, which would, if approved, become an integral part of this Article.

2. Special Districts shall not be proposed or used to avoid, or have the effect of avoiding, compliance with, the standards and requirements of this Article 10-I for Character Districts, and instead, shall be used sparingly and only in exceptional situations.

3. Approval, establishment, and assignment of a Special District shall be subject to approval as a zoning text approval and zoning map amendment.

### SECTION 10-I.3.3 ELEMENTS AND STANDARDS.

Development, re-Development, land, Structures, Buildings, Improvements, and Lots and Building Sites within each District or Civic Space shall include the elements indicated for such District or Civic Space throughout this Article 10-I and shall comply with the applicable District or Civic Space general description and intent thereof and the standards applicable to such District set forth in **Table 10-I.4.2.1 (District & Civic Space Standards)** and elsewhere in this Article 10-I.

## SECTION 10-I.3: DISTRICTS & CIVIC SPACES

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# SECTION 10–I.4

# STANDARDS & REQUIREMENTS

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## SECTION 10–I.4.1

### GENERAL.

Development, re-Development, Lots, Building Sites, Buildings, Structures, and Improvements within Character Districts, Special Districts and Civic Spaces, respectively, shall comply with the applicable standards and requirements referred to in Section 10–I.4.2.

## SECTION 10–I.4.2

### STANDARDS.

#### 10–I.4.2.1 General.

District Standards and Civic Space standards are set forth in **Table 10–I.4.2.1 (District & Civic Space Standards)** and **Table 10–I.4.12.2A (Civic Space Types – Summary)** and **Table 10–I.4.12.2B (Civic Space – Specific Standards)** and elsewhere in this Article 10–I.

#### 10–I.4.2.2 Specific to Civic Space.

Without limitation to Section 10–I.4.2.1, any area designated on the applicable Regulating Plan, or on an approved or proposed Plan as Civic Space shall comply with the following:

1. Civic Space shall be provided in accordance with Sections 10–I.3.2.2 and 10–I.4.12.2, shall comply with **Table 10–I.4.2.1 (District & Civic Space Standards)**, and shall be designed generally as described in **Table 10–I.4.12.2A (Civic Space Types – Summary)**, and **Table 10–I.4.12.2B (Civic Space – Specific Standards)**, as related to the applicable District in which such Civic Space is situated;

2. Without limitation to Section 10–I.4.2.1.1 or 10–I.4.2.1.2, Civic Buildings within Civic Spaces shall comply with the Civic Space standards and requirements of **Table 10–I.4.2.1 (District & Civic Space Standards: CS Civic Space)**, **Table 10–I.4.5.7.1 (Building Types – Summary)**, **Table 10–I.4.5.7.2 (Building Types – Specific Standards)**, and any other requirements as may be determined by the Legislative Body;

3. Buildings operated for Civic purposes but not located within designated Civic Spaces must comply with the applicable District standards of the District in which they are situated.

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**CD-1**

**TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:**  
**NATURAL CHARACTER DISTRICT**



### General Description

The CD-1 Natural Character District consists of natural landscape approximating or reverting to a wilderness condition, including lands unsuitable for settlement due to topography, hydrology, or vegetation. This District typically does not contain buildings; however, small Civic Buildings may be located here, which must be approved by the applicable Decision-Making Authority.



## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**CD-1**

**TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
NATURAL CHARACTER DISTRICT**

### Block Size\*

*\*As Applicable to Development Sites  $\geq$  5 acres of Net Developable Site Area or as Otherwise Included in a Plan*

Block Perimeter

NR

See Table 10-I.4.11.1 (Block Perimeter Standards)

### Private Frontage Types

Common Yard

NA

Stepfront

NA

Porch

NA

Dooryard

NA

Fence

NA

Shopfront

NA

Terrace/Lightwell

NA

Commercialfront

NA

Forecourt

NA

Gallery

NA

Stoop/Stoopfront

NA

Arcade

NA

See Table 10-I.4.5.1 (Private Frontage Types)

### Civic Space Types\*

*\*As Applicable to Development Sites  $\geq$  5 acres of Net Developable Site Area or as Otherwise Included in a Plan*

Natural Area

P

Playground

P

Green

NP

Sport Field or Court

NP

Square

NP

Community Garden

P

Plaza

NP

Pocket Park

P

See Table 10-I.4.12.2A (Civic Space Types – Summary) and Table 10-I.4.12.2B (Civic Space – Specific Standards)

### Permitted Uses

See Table 10-I.4.7.1A (Permitted Principal Uses), Table 10-I.4.7.1B (Permitted Accessory Uses), and Table 10-I.4.7.1C (Permitted Temporary Uses)

### Number of Buildings

Principal Building

1 max.

Accessory Buildings

As approved by applicable Decision-Making Authority

#### LEGEND

The following notations are utilized in this table.



Permitted



Not Permitted



Not Applicable



Required



Not Regulated

# SECTION 10-I.4: STANDARDS & REQUIREMENTS

**CD-1** TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
NATURAL CHARACTER DISTRICT

## Lot Occupation

Lot Width	Per Planning Board
Lot Coverage	Per Planning Board
Lot Frontage	Per Planning Board

## Setbacks

Principal Building		Accessory Building	
Front Setback, Principal Frontage	50 ft. min.	Front Setback, Principal Frontage	50 ft. min.
Front Setback, Secondary Frontage	50 ft. min.	Front Setback, Secondary Frontage	50 ft. min.
Side Setback, each side	50 ft. min.	Side Setback, each side	50 ft. min.
Rear Setback	50 ft. min.	Rear Setback	50 ft. min.

<b>LEGEND</b> The following notations are utilized in this table.	<b>P</b> Permitted	<b>NP</b> Not Permitted	<b>NA</b> Not Applicable	<b>R</b> Required	<b>NR</b> Not Regulated
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## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**CD-1**

**TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:**  
**NATURAL CHARACTER DISTRICT**

### Encroachments – Required Setbacks

Encroachment Type	Front	Side	Rear
Steps to Building Entrance		Per Planning Board	
Open Porches, including steps		Per Planning Board	
Patio Decks, if permitted in applicable Layer		Per Planning Board	
Openwork Fire Balconies		Per Planning Board	
Chimney flues, sills, belt courses, cornices, buttresses eaves and other Architectural Features		Per Planning Board	
Satellite dishes/antennae		Per Planning Board	
Mechanical equipment, including HVAC		Per Planning Board	
Utility lines, wires and associated structures (e.g. poles)		Per Planning Board	
Fences, hedges, walls, Shrubbery, and other landscape features		Per Planning Board	
Parking Areas, Loading Areas and Driveways in 3rd Layer		Per Planning Board	
Stoops/Stoopfronts, Lightwells, Terraces		Per Planning Board	
Balconies and Bay Windows		Per Planning Board	

### Building Standards

#### Building Height\*

Principal Building	Per Planning Board
Accessory Building	Per Planning Board

\*Stories do not include Attics and basements.

#### Ceiling Height

Per Planning Board

#### Facade

Frontage Buildout	Per Planning Board
Entrances/Active Functions	Per Planning Board

SECTION 10-I.4

#### LEGEND

The following notations are utilized in this table.



Permitted



Not Permitted



Not Applicable



Required



Not Regulated

# SECTION 10-I.4: STANDARDS & REQUIREMENTS

CD-1

TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
NATURAL CHARACTER DISTRICT

Building Standards (continued)	
Location of Building at Frontage	Per Planning Board
Facade Position	Per Planning Board
Blank Walls	Per Planning Board
Facade Openings	Per Planning Board
Facade Glazing	Per Planning Board
Window Alignment	Per Planning Board
Facade Articulation	Per Planning Board
Finished Floor Level	Per Planning Board
Facade Window Sill Height	Per Planning Board
Shopfront Frontages	Per Planning Board
Decks	Per Planning Board
Roof Type & Roof Pitch	
Flat	Per Planning Board
Shed	Per Planning Board
Hip	Per Planning Board
Gable	Per Planning Board
Pitch, if any	Per Planning Board

LEGEND					
The following notations are utilized in this table.	<div>P</div> Permitted	<div>NP</div> Not Permitted	<div>NA</div> Not Applicable	<div>R</div> Required	<div>NR</div> Not Regulated

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**CD-1** TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
NATURAL CHARACTER DISTRICT

### Building Types

Tiny Home	NP	Commercial	NP
House	NP	Large Commercial	NP
Duplex	NP	Mixed-Use	NP
Townhouse	NP	Flex	NP
Small Multifamily	NP	Civic*	P
Live/Work	NP	*Subject to approval by applicable Decision-Making Authority	

See Table 10-I.4.5.7.1 (Building Types – Summary) and Table 10-I.4.5.7.2 (Building Types – Specific Standards)

### Vehicular Parking Requirements

Required Vehicular Parking	See Section 10-I.4.8.2
Off-Street Parking Location	Per Planning Board
Garage Location	NP
Driveway to Off-Street Parking & Passenger Drop-off Location	Per Planning Board
Driveway/Vehicular Entrance Maximum Width	Per Planning Board
Parking Structures	NP
Parking Structure Pedestrian Exit Location	NA

### Off-Street Loading, Storage, Trash Receptacle, Utility Box & Service Meter Requirements

Off-Street Loading Space	Per Planning Board
Off-Street Trash Receptacle	Per Planning Board
Off-Street Loading, Trash Receptacle, Utility Box & Service Meter Locations	Per Planning Board

### Thoroughfares and Internal Drives\*

\*As Applicable to Development Sites  $\geq 5$  acres of Net Developable Site Area or as Otherwise Included in a Plan

See Section 10-I.4.11 for additional requirements.

#### LEGEND

The following notations are utilized in this table.

**P** Permitted

**NP** Not Permitted

**NA** Not Applicable

**R** Required

**NR** Not Regulated



## SECTION 10–I.4: STANDARDS & REQUIREMENTS

**CD-1**

**TABLE 10–I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:**  
**NATURAL CHARACTER DISTRICT**

### Non–Building Components

	1st Layer	2nd Layer	3rd Layer
Clothes Drying Apparatus	NA	NA	NA
Heating and Air Conditioning Equipment, Utility, Service and Mechanical Equipment	NP	P	P
Solar Panels	NA	NA	NA
Antennas & Satellite Equipment less than or equal to 18" in diameter	NA	NA	NA
Outdoor Grilling Equipment	NA	NA	NA
Recreation or Play Equipment	P	P	P
Animal Enclosures or Shelters	NA	NA	NA
Swimming Pools, Hot Tubs and Spas	NA	NA	NA

### Private Landscaping and Fencing

#### Landscaping

Per Planning Board

#### Walls & Fencing (not including Screens)

Height	Per Planning Board
Construction	Per Planning Board
Maintenance	Per Planning Board

#### Walls & Fencing Materials

Per Planning Board

#### LEGEND

The following notations are utilized in this table.

**P** Permitted

**NP** Not Permitted

**NA** Not Applicable

**R** Required

**NR** Not Regulated

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**CD-1**

**TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:**  
**NATURAL CHARACTER DISTRICT**

### Screens & Streetscreens

#### Screen & Streetscreen Height

	At Frontage or Adjacent to Civic Space	Not at Frontage or Adjacent to Civic Space
Wall Screen/Streetscreen Height	Per Planning Board	Per Planning Board
Fence Screen/Streetscreen Height	Per Planning Board	Per Planning Board
Hedge Screen/Streetscreen Height	Per Planning Board	Per Planning Board

#### Screen & Streetscreen Materials

Wall Screen or Streetscreen	Per Planning Board
Fence Screen or Streetscreen	Per Planning Board
Hedge Screen or Streetscreen	Per Planning Board

#### Screen & Streetscreen Additional Standards

Any Buildings must be Screened from view of any Thoroughfare	R
Non-Residential & Multi-Family Residential Screen/Streetscreen Adjacent to or across Thoroughfare from Non-Multi-Family Residential	NA
Screening of Parking, Loading Areas, Service Areas, Outdoor Storage, Trash Receptacles/ Dumpsters, HVAC and other equipment Screened from Frontage, Civic Space and Adjacent Property	NA
Rooftop Antenna Screening	NA
HVAC, Mechanical and other Equipment Screening	NA
Streetscreen Location	NA

SECTION 10-I.4

#### LEGEND

The following notations are utilized in this table.

**P** Permitted

**NP** Not Permitted

**NA** Not Applicable

**R** Required

**NR** Not Regulated

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**CD-3**

**TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:**  
**NEIGHBORHOOD CHARACTER DISTRICT**



### General Description

The CD-3 Neighborhood District consists of primarily a Residential area in which Tiny Homes, Houses, and Duplexes are the predominant Building Types. It has medium-to-deep front Setbacks and medium-to-wide side Setbacks. Its Thoroughfares and Internal Drives have Curbs and may include Sidewalks and/or street trees, and form medium-to-large blocks.

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

### CD-3 TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS: NEIGHBORHOOD CHARACTER DISTRICT

#### Block Size\*

*\*As Applicable to Development Sites  $\geq$  5 acres of Net Developable Site Area or as Otherwise Included in a Plan*

**Block Perimeter** 3,000 ft. max.; 800 ft. max. for any side. Pedestrian Path with Bikeway may be used to break up block.

See **Table 10-I.4.11.1 (Block Perimeter Standards)**

#### Private Frontage Types

Common Yard	P	Stepfront	P
Porch	P	Dooryard	P
Fence	P	Shopfront	P **
Terrace/Lightwell	P	Commercialfront	NP
Forecourt	P	Gallery	NP
Stoop/Stoopfront	P	Arcade	NP

See **Table 10-I.4.5.1 (Private Frontage Types)**

\*\*Only if Neighborhood Corner Retail/Service Special Requirement is applicable.

#### Civic Space Types\*

*\*As Applicable to Development Sites  $\geq$  5 acres of Net Developable Site Area or as Otherwise Included in a Plan*

Natural Area***	P	Playground	P
Green	P	Sport Field or Court	P
Square	NP	Community Garden	P
Plaza	NP	Pocket Park	P

See **Table 10-I.4.12.2A (Civic Space Types – Summary)** and **Table 10-I.4.12.2B (Civic Space – Specific Standards)**

\*\*\*Natural Area does not count towards Civic Space Percentage Requirement noted in Section 10-I.3.2.2

#### Permitted Uses

See **Table 10-I.4.7.1A (Permitted Principal Uses)**, **Table 10-I.4.7.1B (Permitted Accessory Uses)**, and **Table 10-I.4.7.1C (Permitted Temporary Uses)**

#### Number of Buildings

**Principal Building** 1 Principal Building max at Frontage; additional Principal Buildings allowed in Third Layer.

**Accessory Buildings** NR

#### LEGEND

The following notations are utilized in this table.

P Permitted

NP Not Permitted

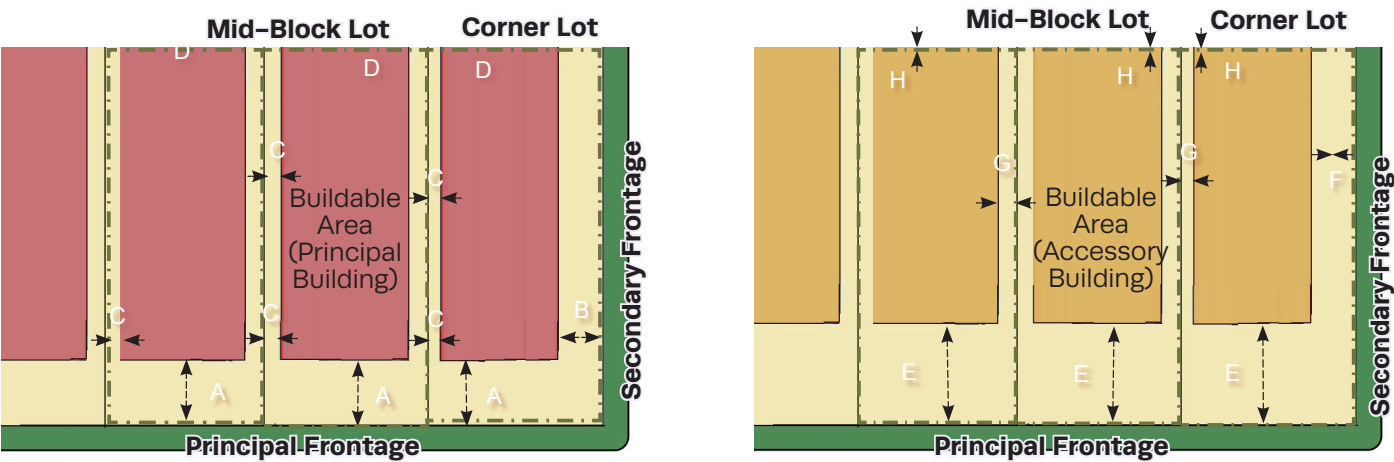
NA Not Applicable

R Required

NR Not Regulated

SECTION 10-I.4: STANDARDS & REQUIREMENTS

CD-3 TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
NEIGHBORHOOD CHARACTER DISTRICT



Lot / Building Site Occupation

Lot / Building Site Width	50 ft. min., 150 ft. max.
Lot / Building Site Coverage	60% max.
Lot / Building Site Frontage	Must enfront a Thoroughfare, except that if Lots or Building Sites have legal and physical access to a Thoroughfare via an Alley, Driveway, or Easement, such Lots or Building Sites may Enfront a Civic Space or Pedestrian Way.

Setbacks

Principal Building			Accessory Building		
Front Setback, Principal Frontage	15 ft. min., 60 ft. max.	A	Front Setback, Principal Frontage	20 ft. min.	E
Front Setback, Secondary Frontage	15 ft. min., 60 ft. max.	B	Front Setback, Secondary Frontage	20 ft. min.	F
Side Setback, each side	12 ft. min.	C	Side Setback, each side	5 ft. min.	G
Rear Setback	12 ft. min.	D	Rear Setback	12 ft. min.	H














































LEGEND					
The following notations are utilized in this table.					
P	Permitted	NP	Not Permitted	NA	Not Applicable
R	Required	NR	Not Regulated		



## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**CD-3**

**TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:**  
**NEIGHBORHOOD CHARACTER DISTRICT**


Encroachments – Required Setbacks			
Encroachment Type	Front	Side	Rear
Steps to Building Entrance	 up to 50% of Setback		
Open Porches, including steps	 up to 50% of Setback		
Patios and Decks, if permitted in applicable Layer	 up to 50% of Setback		
Fire Pit			
Openwork Fire Balconies			
Chimney flues, sills, belt courses, cornices, buttresses eaves and other Architectural Features			
Satellite dishes/antennae			
Mechanical equipment for Building on a Lot of Record existing prior to enactment of this Article, including HVAC	 if Screened from the Frontage		
Mechanical equipment, for Building on a Lot or Building Site created after enactment of this Article, including HVAC			
Utility lines, wires and associated structures (e.g. poles)			
Fences, hedges, walls, Shrubbery, and other landscape features			
Parking Areas, Loading Areas and Driveways on a Lot of Record existing prior to enactment of this Article	 if Screened from the Frontage	 if Screened from the Frontage	
Parking Areas, Loading Areas and Driveways on a Lot or Building Site created after enactment of this Article		 if Screened from the Frontage	
Stoops/Stoopfronts, Lightwells, Terraces	 up to 50% of Setback		
Balconies and Bay Windows			

SECTION 10-I.4

**LEGEND**


The following notations are utilized in this table.

 Permitted

 Not Permitted

 Not Applicable

 Required

 Not Regulated

# SECTION 10-I.4: STANDARDS & REQUIREMENTS

**CD-3** TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
NEIGHBORHOOD CHARACTER DISTRICT

Building Standards	
Building Height*	
Principal Building	2 Stories or 28 ft. max., whichever is less
Accessory Building	2 Stories or 28 ft. max., whichever is less
<i>*Stories do not include Attics, basements, or additional Floodplain elevation requirements as applicable.</i>	
Ceiling Height	
May not exceed 28 ft. in height from finished floor to finished ceiling per Story.	
Facade	
Frontage Buildout	40% min for Lots or Building Sites created after enactment of this Article. Otherwise, <b>NR</b>
Entrances/Active Functions	Main Entrance must be in Facade of Principal Frontage; If Neighborhood Corner Retail/Service Special Requirement is applicable and Shopfront Frontage is at corner, Main Entrance may be at Principal Frontage or at corner.
Blank Walls	<b>NP</b> at Frontage
Facade Glazing	If Neighborhood Corner Retail/Service Special Requirement is applicable, 70% min for Shopfront of total facade area. Otherwise, 20% min – 60% max.
Window Alignment	Except for windows in A-framed Buildings which may have windows horizontally aligned, upper floor windows and other features must be aligned horizontally and vertically with those of first floor. In A-framed Buildings, windows and other features must be aligned horizontally.

LEGEND	<b>P</b>	<b>NP</b>	<b>NA</b>	<b>R</b>	<b>NR</b>
The following notations are utilized in this table.	Permitted	Not Permitted	Not Applicable	Required	Not Regulated

# SECTION 10-I.4: STANDARDS & REQUIREMENTS

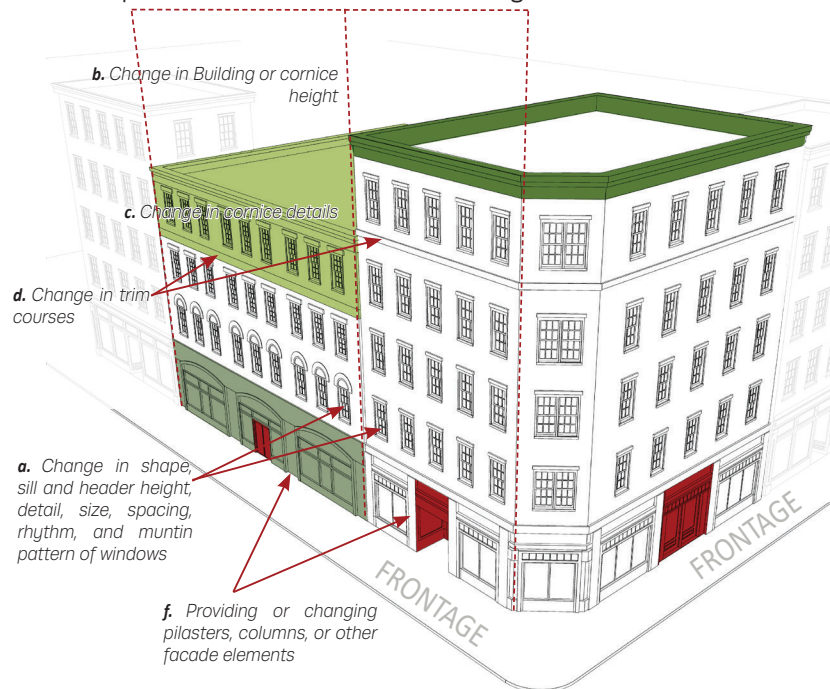
CD-3

TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
NEIGHBORHOOD CHARACTER DISTRICT

## Building Standards (continued)

Any Building having a Facade greater than 100 ft. in width must be differentiated so that it appears to be comprised of two or more Adjacent Buildings, by dividing such Facade into two segments each of which includes a separate entrance and (1) differs from each of the other segments with respect to all of the following: (a) a change in shape, sill, and header height, detail, size, spacing, rhythm, and muntin pattern of windows; (b) a change of Building or cornice height; (c) a change in cornice details; (d) a change of wall material or wall color; a change in trim courses and other horizontal elements; (e) a change in dormer or balcony design, if any, and (f) providing or changing pilasters, columns, or other Facade elements; and (2) is composed with a defined center and edges.

### Facade Articulation



**DISCLAIMER:** The Building in the illustration above cannot be constructed in CD-3 and is only intended to depict Facade elements.

Finished Floor Level	2 ft. – 6 ft. above avg. grade at Frontage Line
Facade Window Sill Height	If the Neighborhood Corner Retail/Service Special Requirement is applicable, at knee wall for Shopfront Frontages. Otherwise, 3 ft. min above avg. grade at Frontage Line
Shopfront Frontages	If the Neighborhood Corner Retail/Service Special Requirement is applicable, 12"–24" knee wall required at Frontage. Otherwise, <b>NA</b> .
Decks	<b>P</b> , subject to compliance with <b>Encroachments – Required Setbacks</b> standards

### LEGEND

The following notations are utilized in this table.

**P** Permitted

**NP** Not Permitted

**NA** Not Applicable

**R** Required

**NR** Not Regulated

SECTION 10-I.4: STANDARDS & REQUIREMENTS

CD-3 TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
NEIGHBORHOOD CHARACTER DISTRICT

Building Standards (continued)	
Roof Type & Roof Pitch	
Flat	NP
Shed	P at rear only and if ridge is attached to an exterior Building wall
Hip	P
Gable, including A-Frame	P
Pitch, if any	8:12 – 14:12, except for shed roofs which may be 3:12 – 14:12

Building Types			
Tiny Home	P	Commercial	NP
House	P	Large Commercial	NP
Duplex	P	Mixed-Use	NP
Townhouse	NP	Flex	NP
Small Multifamily	NP	Civic	P
Live/Work*	P	*Only if Neighborhood Corner Retail/Service Special Requirement is applicable.	

See Table 10-I.4.5.7.1 (Building Types – Summary) and Table 10-I.4.5.7.2 (Building Types – Specific Standards)

LEGEND		P		NP		NA		R		NR	
The following notations are utilized in this table.		Permitted		Not Permitted		Not Applicable		Required		Not Regulated	

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**CD-3**

**TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
NEIGHBORHOOD CHARACTER DISTRICT**

### Vehicular Parking Requirements

Required Vehicular Parking	See Section 10-I.4.8.2
Off-Street Parking Location on Lot of Record existing prior to enactment of this Article	<b>NR</b>
Off-Street Parking Location on Lot or Building Site created after enactment of this Article	<b>P</b> in 3rd Layer only; <b>NP</b> in 1st Layer
Garage or Off-Street Parking Area Location for Lot of Record existing prior to enactment of this Article	<b>NR</b>
Garage or Off-Street Parking Area Location for Lot or Building Site created after enactment of this Article	<b>P</b> in 3rd Layer only; <b>NP</b> in 1st Layer
Parking Lot Location for Lot of Record existing prior to enactment of this Article for Lots with Parking Lot (Commercial) Special Requirement	<b>P</b> in any Layer, provided that Screening is provided on all Lot or Building Site Lines, as provided under "Screens & Streetscreens" below
Driveway to Off-Street Parking & Passenger Drop-off Location	<b>P</b> in any Layer
Driveway/Vehicular Entrance Maximum Width for Lot of Record existing prior to enactment of this Article	<b>NR</b>
Driveway/Vehicular Entrance Maximum Width for Lot or Building Site created after enactment of this Article	10 ft max in 1st Layer
Parking Structures for Lot of Record existing prior to enactment of this Article	<b>NR</b>
Parking Structures for Lot or Building Site created after enactment of this Article	<b>NP</b>
Parking Structure Pedestrian Exit Location for Lot of Record existing prior to enactment of this Article	<b>NR</b>
Parking Structure Pedestrian Exit Location for Lot or Building Site created after enactment of this Article	<b>NA</b>

#### LEGEND

The following notations are utilized in this table.



Permitted



Not Permitted



Not Applicable



Required



Not Regulated

SECTION 10–I.4: STANDARDS & REQUIREMENTS

CD-3 TABLE 10–I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
NEIGHBORHOOD CHARACTER DISTRICT

Off-Street Loading, Storage, Trash Receptacle, Utility Box & Service Meter Requirements

Off-Street Loading Space	<b>R</b> sufficient space, with adequate off-street maneuvering area, to accommodate max # of trucks loading, unloading, or standing at any one time for all Principal Uses of Lot; not to encroach on any required Parking
Off-Street Trash Receptacle	Except for single family Residential, <b>R</b> fully enclosed on 3 sides and enclosed on the 4th side with self-closing gate. In single family Residential, <b>P</b> in 3rd Layer only
Off-Street Loading, Trash Receptacle, Utility Box & Service Meter Locations	<b>P</b> in 3rd Layer only; <b>NP</b> in 1st Layer

Thoroughfares and Internal Drives\*  
*\*As Applicable to Development Sites ≥ 5 acres of Net Developable Site Area or as Otherwise Included in a Plan*

See **Section 10–I.4.11** for additional requirements.

	1st Layer	2nd Layer	3rd Layer
HVAC, Utility, Service & Mechanical Equipment for Lot of Record existing prior to enactment of this Article	<b>P</b> in 1st Layer if Screened from Frontage	<b>P</b> in 2nd Layer if Screened from Frontage	<b>P</b>
HVAC, Utility, Service & Mechanical Equipment for Lot or Building Site created after enactment of this Article	<b>NP</b>	<b>P</b> in 2nd Layer if Screened from Frontage	<b>P</b>
Animal Enclosures or Shelters for Lot of Record without a Principal Building, which Lot & Building existed prior to enactment of this Article	<b>NR</b>	<b>NR</b>	<b>NR</b>
Animal Enclosures or Shelters for Lot or Building Site created after enactment of this Article	<b>NP</b>	<b>NP</b>	<b>P</b>

<b>LEGEND</b> The following notations are utilized in this table.	<b>P</b> Permitted	<b>NP</b> Not Permitted	<b>NA</b> Not Applicable	<b>R</b> Required	<b>NR</b> Not Regulated
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## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**CD-3**

**TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:**  
**NEIGHBORHOOD CHARACTER DISTRICT**

### Private Landscaping and Fencing

#### Landscaping

**NR**

#### Walls & Fencing (not including Screens)

Height	3.5–4 ft. max. at Frontage; otherwise 6 ft. max; height measured from avg. undisturbed grade of Adjacent property at property line
Construction	Finished side must face Adjacent property, Thoroughfare or Internal Drive, Civic Space, Sidewalk or Pedestrian Path
Maintenance	Must be well-maintained, in upright condition and free of missing or broken parts.

#### Walls & Fencing Materials

Natural Wood	<b>P</b>	Chain Link	<b>P</b> at rear sides only
Brick, Natural Stone or Stucco over Masonry	<b>P</b>	Barbed/razor/concertina wire	<b>NP</b>
Wrought Iron or Aluminum	<b>P</b>	Exposed or painted aggregate concrete	<b>NP</b>
Vinyl	<b>P</b>	Smooth or split-faced block	<b>NP</b>

SECTION 10-I.4

#### LEGEND

The following notations are utilized in this table.

**P** Permitted

**NP** Not Permitted

**NA** Not Applicable

**R** Required

**NR** Not Regulated

## SECTION 10–I.4: STANDARDS & REQUIREMENTS

**CD-3**

**TABLE 10–I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
NEIGHBORHOOD CHARACTER DISTRICT**

### Screens & Streetscreens

#### Screen & Streetscreen Height

	At Frontage or in Civic Space	Not at Frontage or in Civic Space
Wall Screen/Streetscreen Height	3.5 ft. to 4 ft.	5 ft. to 6 ft; Min 6 ft. for Dumpsters and trash receptacles
Fence Screen/Streetscreen Height	3.5 ft. to 4 ft.	5 ft. to 6 ft; Min 6 ft. for Dumpsters and trash receptacles
Hedge Screen/Streetscreen Height	3.5 ft. to 4 ft. at installation	5 ft. to 6 ft. at installation

#### Screen & Streetscreen Materials

Wall Screen or Streetscreen	Vinyl, Brick, Natural Stone or Stucco over Masonry
Fence Screen or Streetscreen	Natural Wood, painted or unpainted
Hedge Screen or Streetscreen	Evergreen plants having a foliage density and being spaced to provide a Screen or Streetscreen opacity of a min. 80%

#### Screen & Streetscreen Additional Standards

Non-Residential & Multi-Family Residential Screen/Streetscreen Adjacent to or across Thoroughfare from Non-Multi-Family Residential	<b>NA</b> (Multi-Family not permitted in District)
Screening of Parking, Loading Areas, Service Areas, Outdoor Storage, Trash Receptacles/ Dumpsters, HVAC and other equipment Screened from Frontage, Civic Space and Adjacent Property	<b>R</b> ; except at Driveways. Parking Lots and Parking Areas must be Screened from Frontage and Civic Space by Building or Streetscreen; or by Building, Wall, hedge or fence not at Frontages or in Civic Space.
Rooftop Antenna Screening	<b>R</b>
HVAC, Mechanical and other Equipment Screening	<b>R</b> ; must be Screened from Frontage and Civic Space by Building parapet or other Building Element
Streetscreen Location	On the same plane as the related Building Facade or at the Frontage

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**CD-3**

TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
NEIGHBORHOOD CHARACTER DISTRICT

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SECTION 10-I.4

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**CD-4**

**TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:**  
**NEIGHBORHOOD CENTER CHARACTER DISTRICT**



### General Description

The CD-4 Neighborhood Center District consists of a medium intensity area that has a mix of Building Types and Residential, Commercial, Office and other Uses; there are medium or shallow front Setbacks and narrow side Setbacks; it has variable private landscaping; and it has Thoroughfares and Internal Drives with Curbs, sidewalks, and street trees that define medium-sized blocks.

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**CD-4**

**TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
NEIGHBORHOOD CENTER CHARACTER DISTRICT**

### Block Size\*

*\*As Applicable to Development Sites  $\geq$  5 acres of Net Developable Site Area or as Otherwise Included in a Plan*

**Block Perimeter** 2,000 ft. max., or 3,000 ft. max. if Parking Structure is provided within the Block; 800 ft. max. for any side.

See **Table 10-I.4.11.1 (Block Perimeter Standards)**

### Private Frontage Types

Common Yard	P	Stepfront	P
Porch	P	Dooryard	P
Fence	P	Shopfront	P
Terrace/Lightwell	P	Commercialfront	P
Forecourt	P	Gallery	P
Stoop/Stoopfront	P	Arcade	P

See **Table 10-I.4.5.1 (Private Frontage Types)**

### Civic Space Types\*

*\*As Applicable to Development Sites  $\geq$  5 acres of Net Developable Site Area or as Otherwise Included in a Plan*

Natural Area**	NP	Playground	P
Green	P	Sport Field or Court	P
Square	P	Community Garden	P
Plaza	P	Pocket Park	P

See **Table 10-I.4.12.2A (Civic Space Types – Summary)** and **Table 10-I.4.12.2B (Civic Space – Specific Standards)**

\*\*Natural Area does not count towards Civic Space Percentage Requirement noted in Section 10-I.3.2.2

### Permitted Uses

See **Table 10-I.4.7.1A (Permitted Principal Uses)**, **Table 10-I.4.7.1B (Permitted Accessory Uses)**, and **Table 10-I.4.7.1C (Permitted Temporary Uses)**

### Number of Buildings

**Principal Building** 1 max at Frontage; additional Principal Buildings permitted in Third Layer.

**Accessory Buildings** NR

#### LEGEND

The following notations are utilized in this table.

P Permitted

NP Not Permitted

NA Not Applicable

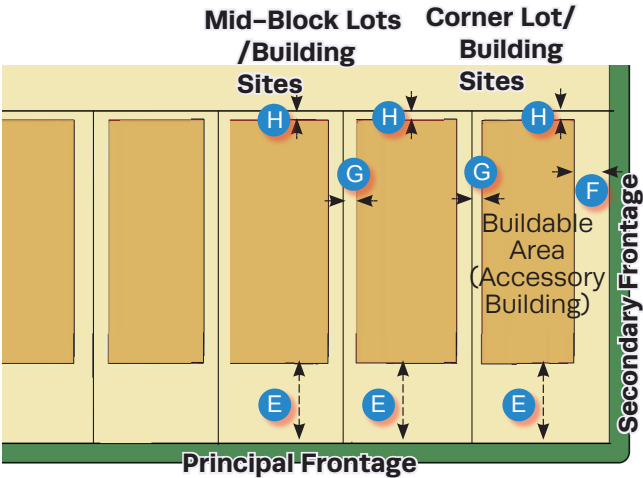
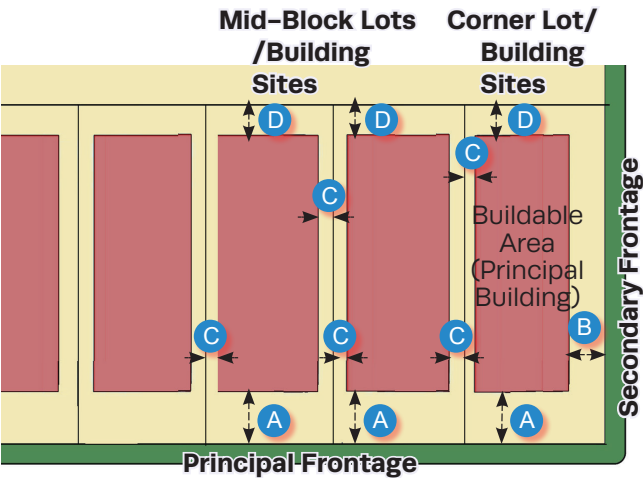
R Required

NR Not Regulated



SECTION 10-I.4: STANDARDS & REQUIREMENTS

CD-4 TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
NEIGHBORHOOD CENTER CHARACTER DISTRICT



Lot / Building Site Occupation

Lot / Building Site Width	25 ft. min., 100 ft. max.
Lot / Building Site Coverage	80% max
Lot / Building Site Frontage	Must enfront a Thoroughfare, except that if Lots or Building Sites have legal and physical access to a Thoroughfare via an Alley, Driveway, or Easement, such Lots or Building Sites may Enfront a Civic Space or Pedestrian Way.

Setbacks





































Principal Building			Accessory Building		
Front Setback, Principal Frontage	0 ft. min, 20 ft. max	A	Front Setback, Principal Frontage	Principal Building Front Setback	E
Front Setback, Secondary Frontage	0 ft. min, 20 ft. max	B	Front Setback, Secondary Frontage	Principal Building Front Setback	F
Side Setback, each side	5 ft. min per side, or 0 ft. for party walls	C	Side Setback, each side	0 ft. min per side	G
Rear Setback	3 ft. min, except 15 ft. min from center line of Rear Alley	D	Rear Setback	3 ft. min, except 15 ft. min from center line of Rear Alley	H

<b>LEGEND</b> The following notations are utilized in this table.		<b>P</b> Permitted	<b>NP</b> Not Permitted	<b>NA</b> Not Applicable	<b>R</b> Required	<b>NR</b> Not Regulated
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## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**CD-4**

**TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
NEIGHBORHOOD CENTER CHARACTER DISTRICT**


Encroachments – Required Setbacks			
Encroachment Type	Front	Side	Rear
Steps to Building Entrance	 up to 50% of Setback		
Open Porches, including steps	 up to 50% of Setback		
Patios and Decks, if permitted in applicable Layer			
Openwork Fire Balconies			
Chimney flues, sills, belt courses, cornices, buttresses eaves and other Architectural Features			
Satellite dishes/antennae			
Mechanical equipment, including HVAC			
Utility lines, wires and associated structures (e.g. poles)			
Fences, hedges, walls, Shrubbery, and other landscape features			
Parking Areas, Loading Areas and Driveways in 3rd Layer			
Stoops/Stoopfronts, Lightwells, Terraces	 up to 50% of Setback		
Balconies and Bay Windows			


SECTION 10-I.4

### LEGEND

The following notations are utilized in this table.

 Permitted

 Not Permitted

 Not Applicable

 Required

 Not Regulated

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**CD-4**

**TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
NEIGHBORHOOD CENTER CHARACTER DISTRICT**

### Building Standards

#### Building Height\*

**Principal Building** 3 Stories max\*\*, 2 Stories min\*\*\*

**Accessory Building** Not to exceed Principal Building Height

\*Stories do not include Attics and basements.

\*\*5% of Block Perimeter may be 4 Stories max.

\*\*\*25% of Block Perimeter may be 1 Story min.

#### Ceiling Height

Ceiling Height may not exceed 14 ft. in height from finished floor to finished ceiling, except for a first floor Commercial function, which must be a minimum of 11 ft. with a maximum of 25 ft. If Building is 2 or more Stories, the first floor must have a minimum ceiling height from finished floor to finished ceiling of 11 ft.

#### Facade

**Frontage Buildout** 60% min

**Entrances/Active Functions** Main Entrance must be in Facade of Principal Frontage. If Shopfront Frontage is at corner, Main Entrance may be at Principal Frontage or at corner.

**Location of Building at Frontage** Parallel to Frontage Lines

**Facade Position** Parallel to straight Frontage Line or to tangent of curved Frontage Line

**Blank Walls**  at Frontage

**Facade Openings** Windows and/or doors spaced ≤ 20 ft. apart  
Square or vertical in proportion  
Except in Shopfront or Gallery Frontage, Windows in Facade and 1st & 2nd Layers must be single-hung, double-hung, casement or awning types.  
In Stories above first, Facade openings must be ≤ 50% of total facade area

**Facade Glazing** 20% min – 70% max for non-Shopfront; 70% min for Shopfront of total facade area

**Window Alignment** Upper floor windows and other features must be aligned with those of first floor.

#### LEGEND

The following notations are utilized in this table.



Permitted



Not Permitted



Not Applicable



Required



Not Regulated

# SECTION 10-I.4: STANDARDS & REQUIREMENTS

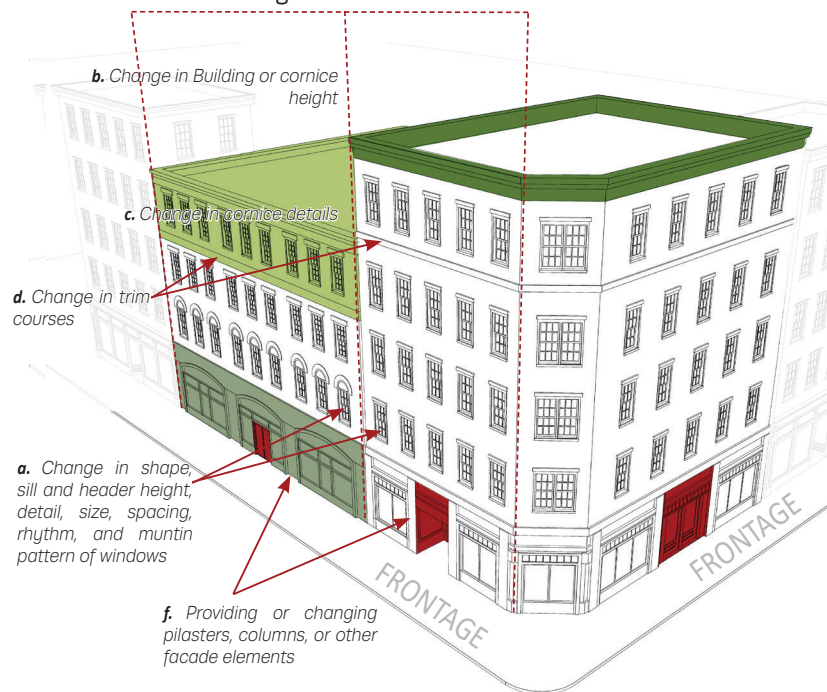
CD-4

TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
NEIGHBORHOOD CENTER CHARACTER DISTRICT

## Building Standards (continued)

Any Facade greater than 100 ft. in width must be differentiated so that it appears to be comprised of two or more Adjacent Buildings, by dividing such Facade into two segments each of which includes a separate entrance and (1) differs from each of the other segments with respect to all of the following: (a) a change in shape, sill, and header height, detail, size, spacing, rhythm, and muntin pattern of windows; (b) a change of Building or cornice height; (c) a change in cornice details; (d) a change of wall material or wall color; a change in trim courses and other horizontal elements; (e) a change in dormer or balcony design, if any, and (f) providing or changing pilasters, columns, or other Facade elements; and (2) is composed with a defined center and edges.

### Facade Articulation



*DISCLAIMER: The 5-Story Building in the illustration above cannot be constructed in CD-4 and is only intended to depict Facade elements.*

Finished Floor Level	If Residential: 2 ft.-6 ft. above avg. grade at Frontage Line
Facade Window Sill Height	If Residential: 3 ft. min above avg. grade at Frontage Line Shopfront Frontages: at knee wall
Shopfront Frontages	12"-24" knee wall required at Frontage
Decks	<b>P</b> , provided that Deck does not extend beyond the Facade of the Building and subject to compliance with <b>Encroachments – Required Setbacks</b> standards

### LEGEND

The following notations are utilized in this table.

**P** Permitted

**NP** Not Permitted

**NA** Not Applicable

**R** Required

**NR** Not Regulated

## SECTION 10–I.4: STANDARDS & REQUIREMENTS

**CD-4**

**TABLE 10–I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:**  
**NEIGHBORHOOD CENTER CHARACTER DISTRICT**

### Building Standards (continued)

#### Roof Type & Roof Pitch

Flat	<b>P</b> for no more than 5% of Block Perimeter
Shed	<b>P</b> at rear only and if ridge is attached to an exterior Building wall
Hip	<b>P</b>
Gable	<b>P</b>
Pitch, if any	8:12 – 14:12, except for shed roofs which may be 3:12 – 14:12

### Building Types

Tiny Home	<b>P</b>	Commercial	<b>P</b>
House	<b>P</b>	Large Commercial*	<b>P</b>
Duplex	<b>P</b>	Mixed-Use	<b>P</b>
Townhouse	<b>P</b>	Flex*	<b>P</b>
Small Multifamily	<b>P</b>	Civic	<b>P</b>
Live/Work	<b>P</b>	*Permitted in Route 1 Special Requirement area only	

See **Table 10–I.4.5.7.1 (Principal Building Types – Summary)** and **Table 10–I.4.5.7.2 (Principal Building Types – Specific Standards)**

### Off-Street Loading, Storage, Trash Receptacle, Utility Box & Service Meter Requirements

Off-Street Loading Space	<b>R</b> sufficient space, with adequate off-street maneuvering area, to accommodate max # of trucks loading, unloading, or standing at any one time for all Principal Uses of Lot; not to encroach on any required Parking
Off-Street Trash Receptacle	Except for single family Residential, <b>R</b> fully enclosed on 3 sides and enclosed on the 4th side with a gate or door. In single family Residential, <b>P</b> in 3rd Layer only
Off-Street Loading, Trash Receptacle, Utility Box & Service Meter Locations	<b>P</b> in 3rd Layer only; <b>NP</b> in 1st Layer



## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**CD-4**

**TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
NEIGHBORHOOD CENTER CHARACTER DISTRICT**

### Thoroughfares and Internal Drives\*

*\*As Applicable to Development Sites  $\geq 5$  acres of Net Developable Site Area or as Otherwise Included in a Plan*

See **Section 10-I.4.11** for additional requirements.

### Vehicular Parking Requirements

Required Vehicular Parking	See Section 10-I.4.8.2
Off-Street Parking Location	<b>P</b> in 3rd Layer only; <b>NP</b> in 1st Layer
Garage Location	<b>P</b> in 3rd Layer only; <b>NP</b> in 1st Layer
Driveway to Off-Street Parking & Passenger Drop-off Location	<b>P</b> in any Layer
Driveway/Vehicular Entrance Maximum Width	10 ft max in 1st Layer if Residential; 24 ft max in 1st Layer if non-Residential or Mixed-Use Building Type
Parking Structures	<b>P</b> subject to requirements of Section 10-I.4.8
Parking Structure Pedestrian Exit Location	Via pedestrian access to Frontage

### Non-Building Components

	1st Layer	2nd Layer	3rd Layer
Clothes Drying Apparatus	<b>P</b>	<b>P</b>	<b>P</b>
Heating and Air Conditioning Equipment, Utility, Service and Mechanical Equipment	<b>NP</b>	<b>P</b> in 2nd Layer if Screened from Frontage	<b>P</b>
Solar Panels	<b>P</b> in 1st or 2nd Layer if parallel to and integrated into roof design.		<b>P</b>
Antennas & Satellite Equipment less than or equal to 18" in diameter	<b>P</b> in 1st or 2nd Layer if such Layer is only location possible for satisfactory reception, as set forth by the FCC.		<b>P</b>
Outdoor Grilling Equipment	<b>P</b>	<b>P</b>	<b>P</b>
Recreation or Play Equipment	<b>P</b>	<b>P</b>	<b>P</b>
Animal Enclosures or Shelters	<b>NP</b>	<b>NP</b>	<b>P</b>
Swimming Pools, Hot Tubs and Spas	<b>NP</b>	<b>NP</b>	<b>P</b>

#### LEGEND

The following notations are utilized in this table.

**P** Permitted

**NP** Not Permitted

**NA** Not Applicable

**R** Required

**NR** Not Regulated

# SECTION 10-I.4: STANDARDS & REQUIREMENTS

**CD-4** TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
NEIGHBORHOOD CENTER CHARACTER DISTRICT

Private Landscaping and Fencing			
Landscaping			
NR			
Walls & Fencing (not including Screens)			
Height		3.5–4 ft. at Frontage; otherwise 6 ft. max; height measured from avg. undisturbed grade of Adjacent property at property line	
Construction		Finished side must face Adjacent property, Thoroughfare, Civic Space, Sidewalk or Pedestrian Path	
Maintenance		Must be well-maintained, in upright condition and free of missing or broken parts.	
Walls & Fencing Materials			
Natural Wood		P	
Brick, Natural Stone or Stucco over Masonry		P	
Wrought Iron or Aluminum		P	
Vinyl		P	
Chain Link		P	at rear sides only
Barbed/razor/concertina wire		NP	
Exposed or painted aggregate concrete		NP	
Smooth or split-faced block		NP	

LEGEND	The following notations are utilized in this table.				
	<div><div>P</div><div>Permitted</div></div>	<div><div>NP</div><div>Not Permitted</div></div>	<div><div>NA</div><div>Not Applicable</div></div>	<div><div>R</div><div>Required</div></div>	<div><div>NR</div><div>Not Regulated</div></div>

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**CD-4**

**TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
NEIGHBORHOOD CENTER CHARACTER DISTRICT**

### Screens & Streetscreens

#### Screen & Streetscreen Height

	At Frontage or Adjacent to Civic Space	Not at Frontage or Adjacent to Civic Space
Wall Screen/Streetscreen Height	3.5 ft. to 4 ft.	5 ft. to 6 ft; Min 6 ft. for Dumpsters and trash receptacles
Fence Screen/Streetscreen Height	3.5 ft. to 4 ft.	5 ft. to 6 ft; Min 6 ft. for Dumpsters and trash receptacles
Hedge Screen/Streetscreen Height	3.5 ft. to 4 ft. at installation	5 ft. to 6 ft. at installation

#### Screen & Streetscreen Materials

Wall Screen or Streetscreen	Vinyl, Brick, Natural Stone or Stucco over Masonry
Fence Screen or Streetscreen	Natural Wood, painted or unpainted
Hedge Screen or Streetscreen	Evergreen plants having a foliage density and being spaced to provide a Screen or Streetscreen opacity of a min. 80%

#### Screen & Streetscreen Additional Standards

Non-Residential & Multi-Family Residential Screen/Streetscreen Adjacent to or across Thoroughfare from Non-Multi-Family Residential	<b>NR</b>
Screening of Parking, Loading Areas, Service Areas, Outdoor Storage, Trash Receptacles/ Dumpsters, HVAC and other equipment Screened from Frontage, Civic Space and Adjacent Property	<b>R</b> ; except at Driveways. Parking Lots and Parking Areas must be Screened from Frontage and Civic Space by Building or Streetscreen; or by Building, Wall, hedge or fence not at Frontages or in Civic Space. Parking Lots and Parking Areas must be Screened from Residential Uses.
Rooftop Antenna Screening	<b>R</b>
HVAC, Mechanical and other Equipment Screening	<b>R</b> ; must be Screened from Frontage and Civic Space by Building parapet or other Building Element
Streetscreen Location	On the same plane as the related Building Facade or at the Frontage

#### LEGEND

The following notations are utilized in this table.

**P** Permitted

**NP** Not Permitted

**NA** Not Applicable

**R** Required

**NR** Not Regulated

# SECTION 10-I.4: STANDARDS & REQUIREMENTS

**SD-AE** TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
AMUSEMENT & ENTERTAINMENT SPECIAL DISTRICT

## General Description

The SD-AE Amusement & Entertainment Special District consists of an area designated for between 50% and 100% that meets all requirements of CD-4 and the remaining area, if any (the “A & E Area”), meeting all requirements of CD-4, modified as noted in this Article 10-I and any additional requirements as may be determined and established by the applicable Decision-Making Authority.

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**SD-AE** TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
AMUSEMENT & ENTERTAINMENT SPECIAL DISTRICT

### Block Size\*

*\*As Applicable to Development Sites  $\geq$  5 acres of Net Developable Site Area or as Otherwise Included in a Plan*

**Block Perimeter** Per CD-4 standard, except **NR** for any A & E Area.

See Table 10-I.4.11.1 (Block Perimeter Standards)

### Private Frontage Types

Common Yard	<b>P</b> *	Stepfront	<b>P</b> *
Porch	<b>P</b> *	Dooryard	<b>P</b> *
Fence	<b>P</b> *	Shopfront	<b>P</b> *
Terrace/Lightwell	<b>P</b> *	Commercialfront	<b>P</b> *
Forecourt	<b>P</b> *	Gallery	<b>P</b> *
Stoop/Stoopfront	<b>P</b> *	Arcade	<b>P</b> *

\*Except in A & E Area, as determined by applicable Decision-Making Authority

See Table 10-I.4.5.1 (Private Frontage Types)

### Civic Space Types\*

*\*As Applicable to Development Sites  $\geq$  5 acres of Net Developable Site Area or as Otherwise Included in a Plan*

Natural Area	<b>P</b> *	Playground	<b>P</b> *
Green	<b>P</b> *	Sport Field or Court	<b>P</b> *
Square	<b>P</b> *	Community Garden	<b>P</b> *
Plaza	<b>P</b> *	Pocket Park	<b>P</b> *

\*Except in A & E Area, as determined by applicable Decision-Making Authority

See Table 10-I.4.12.2A (Civic Space Types – Summary) and Table 10-I.4.12.2B (Civic Space – Specific Standards)

### Permitted Uses

See Table 10-I.4.7.1A (Permitted Principal Uses), Table 10-I.4.7.1B (Permitted Accessory Uses), and Table 10-I.4.7.1C (Permitted Temporary Uses)

### Number of Buildings

**Principal Building** Per CD-4 standard, except per Decision-Making Authority for any A & E Area.

**Accessory Buildings** Per CD-4 standard, except per Decision-Making Authority for any A & E Area.

#### LEGEND

The following notations are utilized in this table.

**P** Permitted

**NP** Not Permitted

**NA** Not Applicable

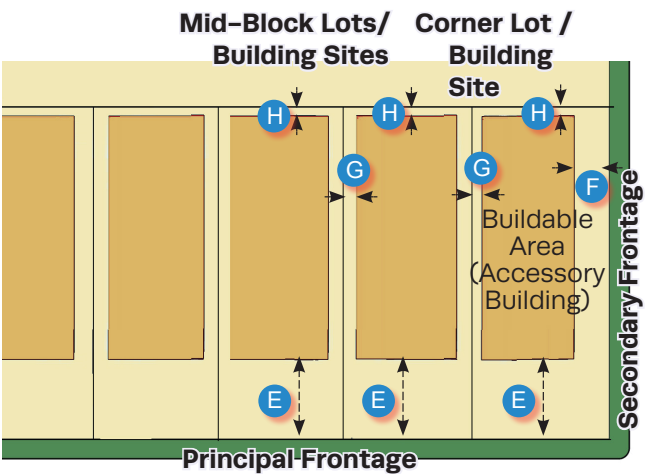
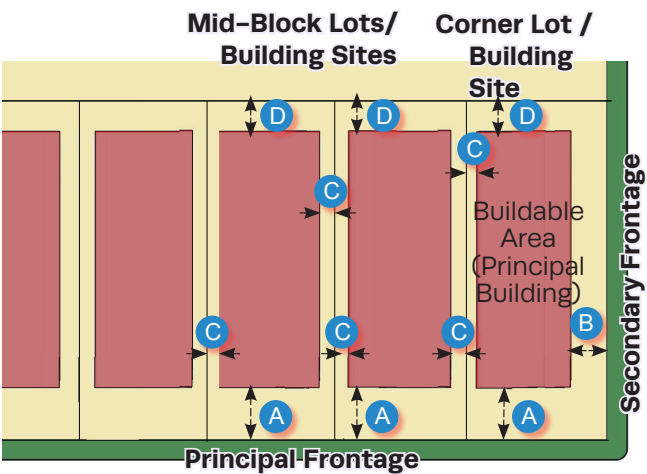
**R** Required

**NR** Not Regulated



SECTION 10-I.4: STANDARDS & REQUIREMENTS

**SD-AE** TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
AMUSEMENT & ENTERTAINMENT SPECIAL DISTRICT



Lot / Building Site Occupation

Lot / Building Site Width	For Flex & Large Commercial Building Types: 150 ft. max. For all other Building Types: Per CD-4 standard, except per Decision-Making Authority for any A & E Area.
Lot / Building Site Coverage	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.
Lot / Building Site Frontage	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.





































Setbacks

Principal Building		Accessory Building	
Front Setback, Principal Frontage	Per CD-4 standard, except per Decision-Making Authority for any A & E Area. <b>A</b>	Front Setback, Principal Frontage	Per CD-4 standard, except per Decision-Making Authority for any A & E Area. <b>E</b>
Front Setback, Secondary Frontage	Per CD-4 standard, except per Decision-Making Authority for any A & E Area. <b>B</b>	Front Setback, Secondary Frontage	Per CD-4 standard, except per Decision-Making Authority for any A & E Area. <b>F</b>
Side Setback, each side	Per CD-4 standard, except per Decision-Making Authority for any A & E Area. <b>C</b>	Side Setback, each side	Per CD-4 standard, except per Decision-Making Authority for any A & E Area. <b>G</b>
Rear Setback	Per CD-4 standard, except <b>NR</b> for any A & E Area. <b>D</b>	Rear Setback	Per CD-4 standard, except <b>NR</b> for any A & E Area. <b>H</b>

LEGEND		P		NP		NA		R		NR	
The following notations are utilized in this table.		Permitted		Not Permitted		Not Applicable		Required		Not Regulated	

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**SD-AE** TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
AMUSEMENT & ENTERTAINMENT SPECIAL DISTRICT

Encroachments – Required Setbacks			
Encroachment Type	Front	Side	Rear
Steps to Building Entrance	 up to 50% of Setback		
Open Porches, including steps	 up to 50% of Setback		
Patio Decks, if permitted in applicable Layer			
Openwork Fire Balconies			
Chimney flues, sills, belt courses, cornices, buttresses eaves and other Architectural Features			
Satellite dishes/antennae			
Mechanical equipment, including HVAC			
Utility lines, wires and associated structures (e.g. poles)			
Fences, hedges, walls, Shrubbery, and other landscape features			
Parking Areas, Loading Areas and Driveways in 3rd Layer			
Stoops/Stoopfronts, Lightwells, Terraces	 up to 50% of Setback		
Balconies and Bay Windows			

LEGEND							
The following notations are utilized in this table.		 Permitted	 Not Permitted	 Not Applicable	 Required	 Not Regulated	

## SECTION 10–I.4: STANDARDS & REQUIREMENTS

**SD-AE** TABLE 10–I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
AMUSEMENT & ENTERTAINMENT SPECIAL DISTRICT

### Building Standards

#### Building Height\*

**Principal Building** Per CD–4 standard, except per Decision–Making Authority for any A & E Area.

**Accessory Building** Per CD–4 standard, except per Decision–Making Authority for any A & E Area.

*\*Stories do not include Attics and basements.*

#### Ceiling Height

Per CD–4 standard, except **NR** for any A & E Area.

#### Facade

**Frontage Buildout** Per CD–4 standard, except **NR** for any A & E Area.

**Entrances/Active Functions** Per CD–4 standard, except **NR** for any A & E Area.

**Location of Building at Frontage** Per CD–4 standard, except **NR** for any A & E Area.

**Facade Position** Per CD–4 standard, except **NR** for any A & E Area.

**Blank Walls** Per CD–4 standard, except **NR** for any A & E Area.

**Facade Openings** Per CD–4 standard, except **NR** for any A & E Area.

**Facade Glazing** Per CD–4 standard, except **NR** for any A & E Area.

**Window Alignment** Per CD–4 standard, except **NR** for any A & E Area.

**Finished Floor Level** Per CD–4 standard, except **NR** for any A & E Area.

**Facade Window Sill Height** Per CD–4 standard, except **NR** for any A & E Area.

**Shopfront Frontages** Per CD–4 standard, except **NR** for any A & E Area.

**Decks** Per CD–4 standard, except **NR** for any A & E Area.

#### LEGEND

The following notations are utilized in this table.



Permitted



Not Permitted



Not Applicable



Required



Not Regulated

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**SD-AE** TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
AMUSEMENT & ENTERTAINMENT SPECIAL DISTRICT

### Building Standards (continued)

#### Roof Type & Roof Pitch

Flat	Per CD-4 standard, except <b>NR</b> for any A & E Area.
Shed	Per CD-4 standard, except <b>NR</b> for any A & E Area.
Hip	Per CD-4 standard, except <b>NR</b> for any A & E Area.
Gable	Per CD-4 standard, except <b>NR</b> for any A & E Area.
Pitch, if any	Per CD-4 standard, except <b>NR</b> for any A & E Area.

### Building Types

Tiny Home	<b>P</b> *	Commercial	<b>P</b> *
House	<b>P</b> *	Large Commercial	<b>P</b> *
Duplex	<b>P</b> *	Mixed-Use	<b>P</b> *
Townhouse	<b>P</b> *	Flex	<b>P</b> *
Small Multifamily	<b>P</b> *	Civic	<b>P</b> *
Live/Work	<b>P</b> *	*Except in A & E Area, as determined by applicable Decision-Making Authority	

See Table 10-I.4.5.7.1 (Principal Building Types – Summary) and Table 10-I.4.5.7.2 (Principal Building Types – Specific Standards)

#### LEGEND

The following notations are utilized in this table.



Permitted



Not Permitted



Not Applicable



Required



Not Regulated

# SECTION 10-I.4: STANDARDS & REQUIREMENTS

**SD-AE** TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
AMUSEMENT & ENTERTAINMENT SPECIAL DISTRICT

## Off-Street Loading, Storage, Trash Receptacle, Utility Box & Service Meter Requirements

Off-Street Loading Space	<b>R</b> sufficient space, with adequate off-street maneuvering area, to accommodate max # of trucks loading, unloading, or standing at any one time for all Principal Uses of Lot; not to encroach on any required Parking
Off-Street Trash Receptacle	Except for single family Residential, <b>R</b> fully enclosed on 3 sides and enclosed on the 4th side with self-closing gate. In single family Residential, <b>P</b> in 3rd Layer only
Off-Street Loading, Trash Receptacle, Utility Box & Service Meter Locations	<b>P</b> in 3rd Layer only; <b>NP</b> in 1st Layer

Thoroughfares and Internal Drives\*

\*As Applicable to Development Sites ≥ 5 acres of Net Developable Site Area or as Otherwise Included in a Plan

See **Section 10-I.4.11** for additional requirements.

## Vehicular Parking Requirements

Required Vehicular Parking	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.
Off-Street Parking Location	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.
Garage Location	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.
Driveway to Off-Street Parking & Passenger Drop-off Location	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.
Driveway/Vehicular Entrance Maximum Width	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.
Parking Structures	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.
Parking Structure Pedestrian Exit Location	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.

<b>LEGEND</b> The following notations are utilized in this table.	<b>P</b> Permitted	<b>NP</b> Not Permitted	<b>NA</b> Not Applicable	<b>R</b> Required	<b>NR</b> Not Regulated
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## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**SD-AE** TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
AMUSEMENT & ENTERTAINMENT SPECIAL DISTRICT

### Non-Building Components

Clothes Drying Apparatus	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.
Heating and Air Conditioning Equipment, Utility, Service and Mechanical Equipment	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.
Solar Panels	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.
Antennas & Satellite Equipment less than or equal to 18" in diameter	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.
Outdoor Grilling Equipment	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.
Recreation or Play Equipment	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.
Animal Enclosures or Shelters	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.
Swimming Pools, Hot Tubs and Spas	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.

### Private Landscaping and Fencing

#### Landscaping

Per CD-4 standard, except per Decision-Making Authority for any A & E Area.

#### Walls & Fencing (not including Screens)

Height	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.
Construction	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.
Maintenance	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.

#### Walls & Fencing Materials

Natural Wood	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.	Chain Link	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.
Brick, Natural Stone or Stucco over Masonry	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.	Barbed wire	NP
Wrought Iron or Aluminum	P	Exposed or painted aggregate concrete	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.
Vinyl	P	Smooth or split-faced block	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.

#### LEGEND

The following notations are utilized in this table.

**P** Permitted

**NP** Not Permitted

**NA** Not Applicable

**R** Required

**NR** Not Regulated

# SECTION 10-I.4: STANDARDS & REQUIREMENTS

**SD-AE** TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
AMUSEMENT & ENTERTAINMENT SPECIAL DISTRICT

Screens & Streetscreens		
Screen & Streetscreen Height		
	At Frontage or Adjacent to Civic Space	Not at Frontage or Adjacent to Civic Space
Wall Screen/Streetscreen Height	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.	
Fence Screen/Streetscreen Height	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.	
Hedge Screen/Streetscreen Height	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.	
Screen & Streetscreen Materials		
Wall Screen or Streetscreen	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.	
Fence Screen or Streetscreen	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.	
Hedge Screen or Streetscreen	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.	
Screen & Streetscreen Additional Standards		
Non-Residential & Multi-Family Residential Screen/Streetscreen Adjacent to or across Thoroughfare from Non-Multi-Family Residential	NR	
Screening of Parking, Loading Areas, Service Areas, Outdoor Storage, Trash Receptacles/ Dumpsters, HVAC and other equipment Screened from Frontage, Civic Space and Adjacent Property	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.	
Rooftop Antenna Screening	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.	
HVAC, Mechanical and other Equipment Screening	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.	
Streetscreen Location	Per CD-4 standard, except per Decision-Making Authority for any A & E Area.	

LEGEND		P		NP		NA		R		NR	
The following notations are utilized in this table.		Permitted		Not Permitted		Not Applicable		Required		Not Regulated	

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**SD-AE**

TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
AMUSEMENT & ENTERTAINMENT SPECIAL DISTRICT

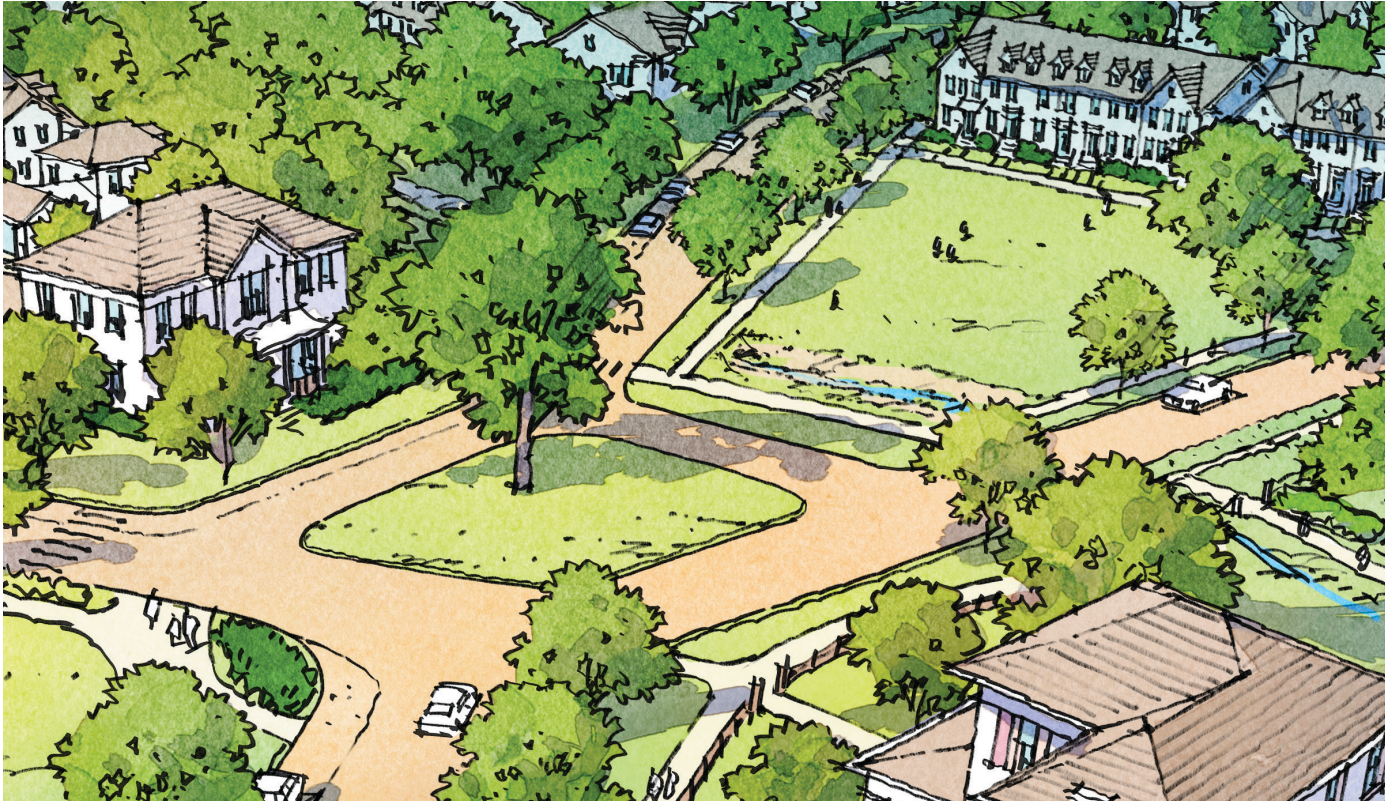
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SECTION 10-I.4

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

CS

TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
CIVIC SPACE



### General Description

Civic Space is Open Area designed as one or more of the Civic Space Types allowed within the applicable District as provided in **Table 10-I.4.12.2A (Civic Space Types – Summary)** and **Table 10-I.4.12.2B (Civic Space – Specific Standards)**, as well as sites dedicated to significant Buildings to be used for Civic purposes, such as Libraries, Museums, and Schools, excluding areas, Buildings, and Structures used governmental purposes. Civic Space and Improvements in Civic Space shall comply with this **Table 10-I.4.2.1 (District & Civic Space Standards: CS Civic Space)**, **Table 10-I.4.12.2A (Civic Space Types – Summary)** and **Table 10-I.4.12.2B (Civic Space – Specific Standards)**.

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**CS**

**TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:**  
**CIVIC SPACE**

### Block Size\*

*\*As Applicable to Development Sites  $\geq$  5 acres of Net Developable Site Area or as Otherwise Included in a Plan*

Block Perimeter Per Standard of any adjacent District

See Table 10-I.4.11.1 (Block Perimeter Standards)

### Private Frontage Types

**NR**

### Civic Space Types\*

*\*As Applicable to Development Sites  $\geq$  5 acres of Net Developable Site Area or as Otherwise Included in a Plan*

Natural Area	<b>P</b>	Playground	<b>P</b>
Green	<b>P</b>	Sport Field or Court	<b>P</b>
Square	<b>P</b>	Community Garden	<b>P</b>
Plaza	<b>P</b>	Pocket Park	<b>P</b>

See Table 10-I.4.12.2A (Civic Space Types – Summary) and Table 10-I.4.12.2B (Civic Space – Specific Standards)

### Permitted Uses

See Table 10-I.4.7.1A (Permitted Principal Uses), Table 10-I.4.7.1B (Permitted Accessory Uses), and Table 10-I.4.7.1C (Permitted Temporary Uses)

### Number of Buildings

Principal Building **NR**

Accessory Buildings **NR**

SECTION 10-I.4

#### LEGEND

The following notations are utilized in this table.

**P** Permitted

**NP** Not Permitted

**NA** Not Applicable

**R** Required

**NR** Not Regulated



# SECTION 10–I.4: STANDARDS & REQUIREMENTS

CS

TABLE 10–I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
CIVIC SPACE

Lot / Building Site Occupation	
Lot / Building Site Width	NR
Lot / Building Site Coverage	NR
Lot / Building Site Frontage	Must enfront a vehicular Thoroughfare, Pedestrian Path, Passage, pedestrian walkway or waterbody

Setbacks			
Principal Building		Accessory Building	
Front Setback, Principal Frontage	0 ft. min., 50% of Lot Depth max.	Front Setback, Principal Frontage	NR
Front Setback, Secondary Frontage	0 ft min.	Front Setback, Secondary Frontage	NR
Side Setback, each side	0 ft. min.	Side Setback, each side	NR
Rear Setback	0 ft. min.	Rear Setback	NR

LEGEND	The following notations are utilized in this table.				
	<div>P</div> Permitted	<div>NP</div> Not Permitted	<div>NA</div> Not Applicable	<div>R</div> Required	<div>NR</div> Not Regulated



## SECTION 10-I.4: STANDARDS & REQUIREMENTS

CS

TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
CIVIC SPACE

### Encroachments – Required Setbacks

Encroachment Type	Front	Side	Rear
Steps to Building Entrance	P up to 50% of Setback	P	P
Open Porches, including steps	P up to 50% of Setback	P	P
Patio Decks, if permitted in applicable Layer	NP	P	P
Openwork Fire Balconies	NP	P	P
Chimney flues, sills, belt courses, cornices, buttresses eaves and other Architectural Features	P	P	P
Satellite dishes / antennae	P	P	P
Mechanical equipment, including HVAC	NP	P	P
Utility lines, wires and associated structures (e.g. poles)	P	P	P
Fences, hedges, walls, Shrubbery, and other landscape features	P	P	P
Parking Areas, Loading Areas and Driveways in 3rd Layer	NP	P	P
Stoops/Stoopfronts, Lightwells, Terraces	P up to 50% of Setback	P	P
Balconies and Bay Windows	P	P	P

SECTION 10-I.4

#### LEGEND

The following notations are utilized in this table.



Permitted



Not Permitted



Not Applicable



Required



Not Regulated

## SECTION 10–I.4: STANDARDS & REQUIREMENTS

CS

TABLE 10–I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
CIVIC SPACE

### Building Standards

#### Building Height\*

Principal Building	20 ft. higher than max. Height of any adjacent Character District, max.
Accessory Building	Not to exceed Principal Building Height

\*Stories do not include Attics and basements.

#### Ceiling height

14 ft. min. 1st floor; 10 ft. min. other floors

#### Facade

Entrances/Active Functions	Main Entrance must be in Facade of Principal Frontage.
Location of Building at Frontage	Parallel to Frontage Lines
Facade Position	Parallel to straight Frontage Line or to tangent of curved Frontage Line
Blank Walls	<b>NP</b> at Frontage
Facade Openings	Windows and/or doors spaced $\leq$ 20 ft. apart Square or vertical in proportion Except in Shopfront or Gallery Frontage, Windows in Facade and 1st & 2nd Layers must be single-hung, double-hung, casement or awning types. In Stories above first, Facade openings must be $\leq$ 50% of total facade area
Facade Glazing	20% min., 70% max.
Window Alignment	Upper floor windows and other features must be aligned with those of first floor.
Finished Floor Level	<b>NR</b>
Facade Window Sill Height	<b>NR</b>
Shopfront Frontages	<b>NR</b>
Decks	<b>NP</b> at Frontage

#### Roof Type & Roof Pitch

Flat	<b>P</b> for no more than 5% of Block Perimeter
Shed	<b>P</b> at rear only and if ridge is attached to an exterior Building wall
Hip	<b>P</b>
Gable	<b>P</b>
Pitch, if any	8:12 – 14:12, except for shed roofs which may be 3:12 – 14:12

#### LEGEND

The following notations are utilized in this table.

**P** Permitted

**NP** Not Permitted

**NA** Not Applicable

**R** Required

**NR** Not Regulated

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

CS

**TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:**  
CIVIC SPACE

### Building Types

Tiny Home	NP	Live/Work	NP
House	NP	Commercial	NP
Duplex	NP	Mixed-Use	NP
Townhouse	NP	Flex	NP
Small Multifamily	NP	Civic	P

See **Table 10-I.4.5.7.1 (Building Types – Summary)** and **Table 10-I.4.5.7.2 (Building Types – Specific Standards)**

### Vehicular Parking Requirements

Required Vehicular Parking	See Section 10-I.4.8.2
Off-Street Parking Location	P in 3rd Layer only; NP in 1st Layer
Garage Location	P in 3rd Layer only; NP in 1st Layer
Driveway to Off-Street Parking & Passenger Drop-off Location	P in any Layer
Driveway/Vehicular Entrance Maximum Width	24 ft max in 1st Layer and 2nd Layer
Parking Structures	P subject to requirements of Section 10-I.4.8
Parking Structure Pedestrian Exit Location	Via pedestrian access to Frontage

### Off-Street Loading, Storage, Trash Receptacle, Utility Box & Service Meter Requirements

Off-Street Loading Space	R sufficient space, with adequate off-street maneuvering area, to accommodate max # of trucks loading, unloading, or standing at any one time for all Principal Uses of Lot; not to encroach on any required Parking
Off-Street Trash Receptacle	P in 3rd Layer only
Off-Street Loading, Trash Receptacle, Utility Box & Service Meter Locations	P in 3rd Layer only; NP in 1st Layer

### Thoroughfares and Internal Drives\*

*\*As Applicable to Development Sites  $\geq$  5 acres of Net Developable Site Area or as Otherwise Included in a Plan*

See **Section 10-I.4.11** for additional requirements.

#### LEGEND

The following notations are utilized in this table.



Permitted



Not Permitted



Not Applicable



Required



Not Regulated

## SECTION 10–I.4: STANDARDS & REQUIREMENTS

**CS**

**TABLE 10–I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:**  
**CIVIC SPACE**

### Non–Building Components

	1st Layer	2nd Layer	3rd Layer
Clothes Drying Apparatus	NP	NP	NP
Heating and Air Conditioning Equipment, Utility, Service and Mechanical Equipment	NP	P in 2nd Layer if Screened from Frontage	P
Solar Panels	P in 1st or 2nd Layer if parallel to and integrated into roof design.		P
Antennas & Satellite Equipment less than or equal to 18" in diameter	P in 1st or 2nd Layer if such Layer is only location possible for satisfactory reception, as set forth by the FCC.		P
Outdoor Grilling Equipment	NP	NP	P
Recreation or Play Equipment	P	P	P
Animal Enclosures or Shelters	NP	NP	P
Swimming Pools, Hot Tubs and Spas	P	P	P

#### LEGEND

The following notations are utilized in this table.



Permitted



Not Permitted



Not Applicable



Required



Not Regulated

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

CS

TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
CIVIC SPACE

### Private Landscaping and Fencing

#### Landscaping

NR

#### Walls & Fencing (not including Screens)

Height	3.5–4 ft. at Frontage; otherwise 6 ft. max; height measured from avg. undisturbed grade of Adjacent property at property line
Construction	Finished side must face Adjacent property, Thoroughfare, Civic Space, Sidewalk or Pedestrian Path
Maintenance	Must be well-maintained, in upright condition and free of missing or broken parts.

#### Walls & Fencing Materials

Natural Wood	P	Chain Link	P at rear sides only
Brick, Natural Stone or Stucco over Masonry	P	Barbed/razor/concertina wire	NP
Wrought Iron or Aluminum	P	Exposed or painted aggregate concrete	NP
Vinyl	P	Smooth or split-faced block	NP

SECTION 10-I.4

#### LEGEND

The following notations are utilized in this table.



Permitted



Not Permitted



Not Applicable



Required



Not Regulated

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

CS

TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
CIVIC SPACE

### Screens & Streetscreens

#### Screen & Streetscreen Height

	At Frontage or Adjacent to Civic Space	Not at Frontage or Adjacent to Civic Space
Wall Screen/Streetscreen Height	3.5 ft. to 4 ft.	5 ft. to 6 ft; Min 6 ft. for Dumpsters and trash receptacles
Fence Screen/Streetscreen Height	3.5 ft. to 4 ft.	5 ft. to 6 ft; Min 6 ft. for Dumpsters and trash receptacles
Hedge Screen/Streetscreen Height	3.5 ft. to 4 ft. at installation	5 ft. to 6 ft. at installation

#### Screen & Streetscreen Materials

Wall Screen or Streetscreen	Vinyl, Brick, Natural Stone or Stucco over Masonry
Fence Screen or Streetscreen	Natural Wood, painted or unpainted
Hedge Screen or Streetscreen	Evergreen plants with min. 80% opacity

#### Screen & Streetscreen Additional Standards

Non-Residential & Multi-Family Residential Screen/Streetscreen Adjacent to or across Thoroughfare from Non-Multi-Family Residential	NA (Multi-Family not permitted in District)
Screening of Parking, Loading Areas, Service Areas, Outdoor Storage, Trash Receptacles/ Dumpsters, HVAC and other equipment Screened from Frontage, Civic Space and Adjacent Property	R; except at Driveways. Parking Lots and Parking Areas must be Screened from Frontage and Civic Space by Building or Streetscreen; or by Building, Wall, hedge or fence not at Frontages or Adjacent to Civic Space.
Rooftop Antenna Screening	R
HVAC, Mechanical and other Equipment Screening	R; must be Screened from Frontage and Civic Space by Building parapet or other Building Element
Streetscreen Location	On the same plane as the related Building Facade or at the Frontage

#### LEGEND

The following notations are utilized in this table.



Permitted



Not Permitted



Not Applicable



Required



Not Regulated



## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**CS**

TABLE 10-I.4.2.1 DISTRICT & CIVIC SPACE STANDARDS:  
CIVIC SPACE

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SECTION 10-I.4

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

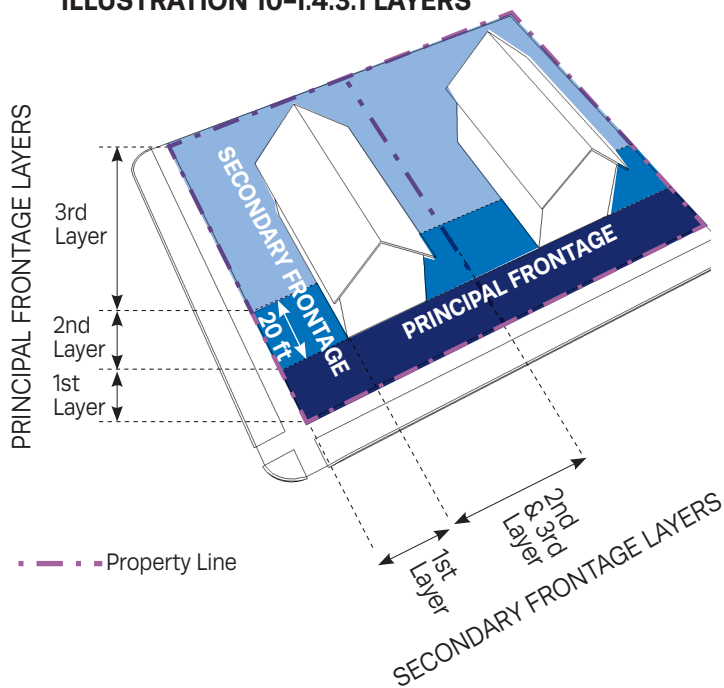
### SECTION 10-I.4.3 LOTS & BUILDING SITES.

#### 10-I.4.3.1 Layers.

Lots and Building Sites are composed of three Layers, the First Layer, the Second Layer and the Third Layer, as shown in **Illustration 10-I.4.3.1 (Layers)** and as defined in Section 10-I.4.

Layers are used to describe areas within a Lot or Building Site where certain elements or Uses are permitted to be located.

ILLUSTRATION 10-I.4.3.1 LAYERS



#### 10-I.4.3.2 Lot / Building Site Dimensions.

Newly platted Lots or Building Sites within each District shall be dimensioned according to **Table 10-I.4.2.1 (District & Civic Space Standards)**.

#### 10-I.4.3.3 Lot / Building Site Coverage.

Lot or Building Site Coverage within each District shall not exceed that provided in **Table 10-I.4.2.1 (District & Civic Space Standards)**.

#### 10-I.4.3.4 Lot / Building Site Enfrontment.

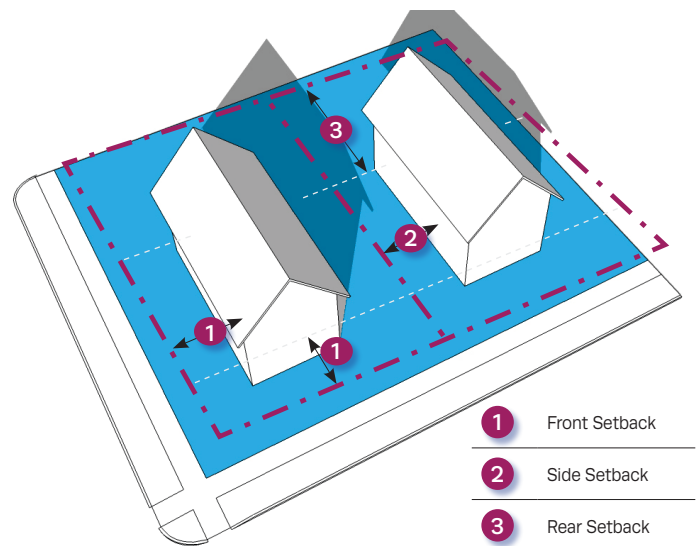
Each Lot or Building Site shall have comply with the Lot or Building Site Enfrontment requirements of **Table 10-I.4.2.1 (District & Civic Space Standards)**.

### SECTION 10-I.4.4 BUILDING PLACEMENT, NUMBER & FRONT FACADES.

#### 10-I.4.4.1 Building Placement.

Buildings shall be located in relation to the boundaries of their Lots or Building Sites and shall comply with the Setback standards. See **Illustration 10-I.4.4.1 (Setback Designations)** within each District according to **Table 10-I.4.2.1 (District & Civic Space Standards)**.

ILLUSTRATION 10-I.4.4.1 SETBACK DESIGNATIONS

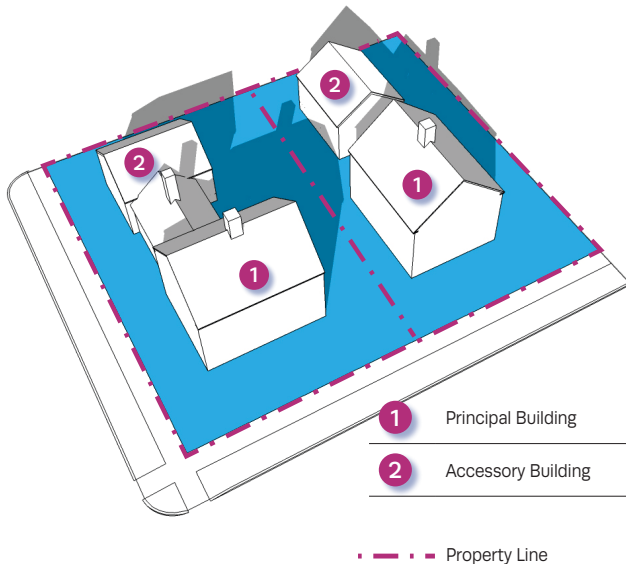


## 10-I.4.4.2 Principal Buildings.

Within each District:

1. the number of Principal Buildings on each Lot or Building Site shall not exceed that indicated in **Table 10-I.4.2.1 (District & Civic Space Standards)**; and
2. Principal Buildings shall be oriented and situated generally as specified in **Table 10-I.4.2.1 (District & Civic Space Standards)** and illustrated generally in **Illustration 10-I.4.4.2 (Principal Building / Accessory Building)**.

**ILLUSTRATION 10-I.4.4.2 PRINCIPAL BUILDING / ACCESSORY BUILDING**



## 10-I.4.4.4 Building Facades.

1. Within each District, Building Facades shall be built as required in **Table 10-I.4.2.1 (District & Civic Space Standards)**.
2. In each District, the Facade of the Principal Building shall be built along the minimum percentage of the Front Lot or Building Site Line width at the Setback, as specified as Frontage Buildout on **Table 10-I.4.2.1 (District & Civic Space Standards)**; provided that a Streetscreen may be substituted for a Facade for up to 20% of such applicable Frontage Buildout requirement.

## 10-I.4.4.3 Accessory Building.

Within each District:

1. the number of Accessory Buildings on each Lot or Building Site shall not exceed that indicated in **Table 10-I.4.2.1 (District & Civic Space Standards)**; and
2. any such Accessory Buildings shall be located as specified in **Table 10-I.4.2.1 (District & Civic Space Standards)** and illustrated generally in **Illustration 10-I.4.4.2 (Principal Building / Accessory Building)**.

## SECTION 10–I.4: STANDARDS & REQUIREMENTS

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### SECTION 10–I.4.5

#### PRIVATE FRONTAGE, BUILDING FORM & BUILDING TYPE; FLOOR ELEVATION.

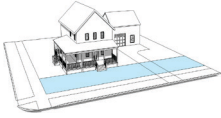
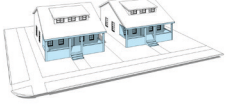
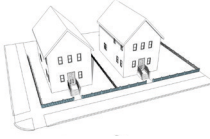


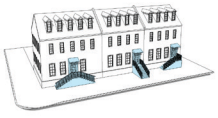

##### 10–I.4.5.1 Private Frontage – General.

Within each District, the Private Frontage shall conform to and be allocated in accordance with **Table 10–I.4.5.1 (Private Frontage Types)** and **Table 10–I.4.2.1 (District & Civic Space Standards)**.

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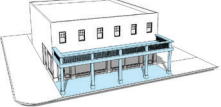
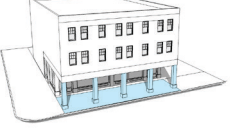
## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**TABLE 10-I.4.5.1 PRIVATE FRONTAGE TYPES**

Private Frontage Type	Permitted Districts
 <p><b>Common Yard</b> A planted Frontage wherein the Facade is set back substantially from the Frontage Line. The front yard created remains unfenced and is visually continuous with Adjacent yards, supporting a common landscape. The deep Setback provides a buffer from higher speed rights-of-way.</p>	<p>CD-3 CD-4 SD-AE, as indicated in Table 10-I.4.2.1</p>
 <p><b>Porch</b> A planted Frontage wherein the Facade is set back from the Frontage Line with an attached porch permitted to Encroach. This may be used with or without a Fence to maintain right-of-way spatial definition.</p>	<p>CD-3 CD-4 SD-AE, as indicated in Table 10-I.4.2.1</p>
 <p><b>Fence</b> A Frontage wherein the Facade is set back from the Frontage Line, and the front yard is separated by a Fence. This may be used with or without a Porch.</p>	<p>CD-3 CD-4 SD-AE, as indicated in Table 10-I.4.2.1</p>
 <p><b>Terrace/Lightwell</b> A Frontage where the Facade is set back from the Frontage line by an elevated landing and a sunken Lightwell. This type buffers Residential or Commercial Uses from urban Sidewalks and allows for activation of below grade space.</p>	<p>CD-3 CD-4 SD-AE, as indicated in Table 10-I.4.2.1</p>
 <p><b>Forecourt</b> A Frontage wherein a portion of the Facade is close to the Frontage Line and the central portion is set back. The Forecourt creates an intimate gathering space for the residents or additional shopping or Restaurant seating area within retail and service areas. This type should be used sparingly and may be allocated in conjunction with other Frontage types.</p>	<p>CD-3 CD-4 SD-AE, as indicated in Table 10-I.4.2.1</p>
 <p><b>Stoop/Stoopfront</b> A Frontage wherein the Facade is aligned close to the Frontage Line with the first Story elevated from the Sidewalk sufficiently to secure privacy for the windows. The entrance is usually an exterior stair and landing. This type is recommended for ground-floor Residential Use.</p>	<p>CD-3 CD-4 SD-AE, as indicated in Table 10-I.4.2.1</p>
 <p><b>Stepfront</b> A Frontage wherein the Facade is aligned very close to the Frontage Line. The entrance is usually an exterior single step without a landing. This type is recommended for ground-floor Residential or Commercial Use.</p>	<p>CD-3 CD-4 SD-AE, as indicated in Table 10-I.4.2.1</p>

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

TABLE 10-I.4.5.1 PRIVATE FRONTAGE TYPES

Private Frontage Type	Permitted Districts
 <p><b>Dooryard</b> A Frontage type where the front Facade of the Building is set back a small distance and the Frontage line is defined by a low wall, decorative Fence or hedge, creating a small dooryard. The dooryard shall not provide public circulation along a right-of-way. The dooryard may be raised, sunken, or at grade and is intended for ground-floor Residential.</p>	<p>CD-3 CD-4 SD-AE, as indicated in Table 10-I.4.2.1</p>
 <p><b>Shopfront</b> A Frontage wherein the Facade is aligned close to or at the Frontage Line with the Building entrance at Sidewalk grade. This type is conventional for Commercial Use. It has a substantial glazing on the Sidewalk level and may include an awning that may overlap the sidewalk.</p>	<p>CD-3* CD-4 SD-AE, as indicated in Table 10-I.4.2.1</p>
 <p><b>Commercialfront</b> A Frontage wherein the Facade is aligned close to or at the Frontage Line with the Building entrance at or above Sidewalk grade. This type is conventional for Office or Commercial Use. It may have a substantial glazing on the Facade</p>	<p>CD-4 SD-AE, as indicated in Table 10-I.4.2.1</p>
 <p><b>Gallery</b> A Frontage wherein the Facade is aligned close to the Frontage line with an attached cantilevered shed or a lightweight colonnade. This type is intended for buildings with ground floor Commercial Uses and the Gallery may be one or two Stories.</p>	<p>CD-4 SD-AE, as indicated in Table 10-I.4.2.1</p>
 <p><b>Arcade</b> A colonnade supporting habitable space where the front Facade is set back from the Frontage line and the arcade meets the Sidewalk. An Encroachment permit must be obtained for any part of the Building which extends into the public right-of-way.</p>	<p>CD-4 SD-AE, as indicated in Table 10-I.4.2.1</p>

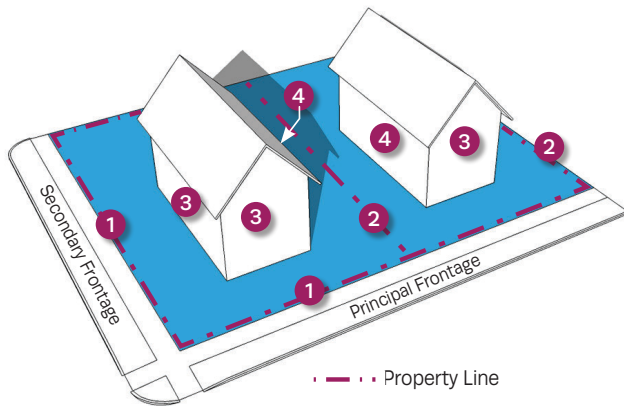
\*Only if Neighborhood Corner Retail/Service Special Requirement is applicable.



## 10-I.4.5.2 Private Frontage on Corner Lots or Building Sites.

- Each Lot or Building Site that abuts two (2) Thoroughfares has more than one (1) Private Frontage: (a) one Principal Frontage and (b) one or more Secondary Frontages, depending on the number of Thoroughfares abutted by such Lot. See generally **Illustration 10-I.4.5.2 (Frontage & Lot or Building Site Lines)**.

**ILLUSTRATION 10-I.4.5.2 FRONTAGE & LOT OR BUILDING SITE LINES**



- |   |                          |
|---|--------------------------|
| 1 | Frontage Line            |
| 2 | Lot / Building Site Line |
| 3 | Facades                  |
| 4 | Facades                  |

- For Lots or Building Sites having more than one Private Frontage, each Frontage shall comply with all applicable Frontage requirements; provided that standards for the Second Layer and Third Layer pertain only to the Principal Frontage and prescriptions for the First Layer pertain to both Frontages. See **Illustration 10-I.4.3.1 (Layers)**.

- No Through Lot shall be created after the date this Article 10-I was adopted.

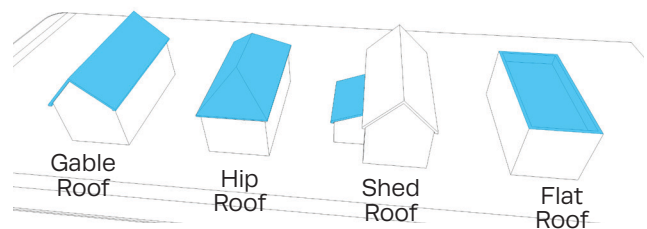
## 10-I.4.5.3 Building Height & Ceiling Height; Finished Floor Elevation.

- Building Heights and ceiling heights within each District shall conform to **Table 10-I.4.2.1 (District & Civic Space Standards)**.
- In calculating Building Height, none of the following shall be considered:
  - Attics under 14 ft in height at their tallest point;
  - Raised Basements; or
  - Masts, belfries, clock towers, chimney flues, silos, water tanks, or elevator bulkheads.
- Building ground finish floor elevation within each District shall conform to **Table 10-I.4.2.1 (District & Civic Space Standards)**.

## 10-I.4.5.4 Roof Type & Pitch.

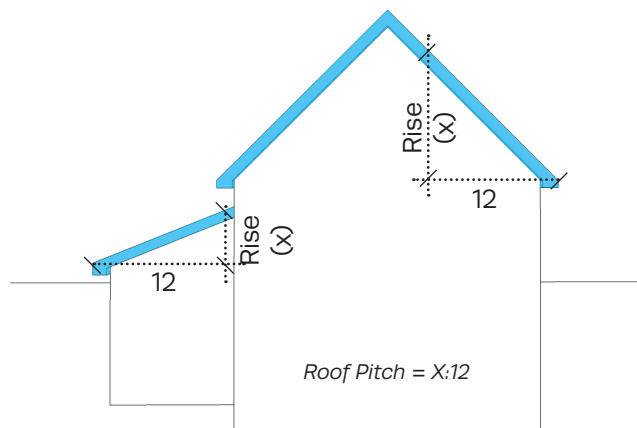
- Within each District, Building roof type and pitch shall conform to **Table 10-I.4.2.1 (District & Civic Space Standards)**.

**ILLUSTRATION 10-I.4.5.4.1 ROOF TYPES**



## SECTION 10-I.4: STANDARDS & REQUIREMENTS

ILLUSTRATION 10-I.4.5.4.2 ROOF PITCH



### 10-I.4.5.5 Habitable Area of Accessory Dwelling.

The habitable area of an Accessory Dwelling within an Accessory Building shall be in compliance with Section 7.17 of the Town Zoning Ordinance.

### 10-I.4.5.6 Screens and Streetscreens.

1. Any Screen or Streetscreen shall comply with **Table 10-I.4.2.1 (District & Civic Space Standards)**.
2. Screens shall have openings no larger than necessary to allow automobile and pedestrian access.
3. Any Streetscreen shall be built on the same plane as the related Building Facade or at the Frontage Line.







### 10-I.4.5.7 Building Types.

Buildings in each District shall be of one or more of the Building Types specified for such District in **Table 10-I.4.5.7.1 (Building Types – Summary)** and **Table 10-I.4.5.7.2 (Building Types – Specific Standards)**.

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# SECTION 10-I.4: STANDARDS & REQUIREMENTS






**TABLE 10-I.4.5.7.1 BUILDING TYPES – SUMMARY**

Building Type	Permitted Districts
 <p><b>Tiny Home</b> A detached Building not exceeding 400 square feet interior usable area, having an Edgeyard or Sideyard yard type that may be shared with an Accessory Building in the backyard. This Building type typically incorporates a single Residential unit.</p>	<p>CD-3 CD-4 SD-AE, as indicated in Table 10-I.4.2.1</p>
 <p><b>House</b> A detached Building of more than 400 square feet of interior usable area, having an Edgeyard yard type that may be shared with an Accessory Building in the backyard. This Building type typically incorporates a single Residential unit, but may include a maximum of two (2) units per Building.</p>	<p>CD-3 CD-4 SD-AE, as indicated in Table 10-I.4.2.1</p>
 <p><b>Duplex</b> A small- to medium-sized Building with separate entrances at least one of which faces the right-of-way. Both of the units, side-by-side, front-to-back or over-and-under, are contained within a single Building massing. This Building type sits on a small-to-medium sized urban Lot or Building Site, having an Edgeyard or Sideyard yard type that may be shared with an Accessory Building in the backyard. This Building type typically incorporates two Residential units, but may include one or more additional Residential units as allowed or required by the Town Zoning Ordinance per Maine LD 2003.</p>	<p>CD-3 CD-4 SD-AE, as indicated in Table 10-I.4.2.1</p>
 <p><b>Townhouse</b> A Principal Building in a collection of very narrow to medium sized attached Buildings with a Rearyard yard type (or Sideyard as an end unit) that consists of side-by-side units with individual entries facing the right-of-way. This type shares at least one party wall with another Building of the same type and occupies the full Frontage Line.</p>	<p>CD-4 SD-AE, as indicated in Table 10-I.4.2.1</p>
 <p><b>Small Multi-Family Building</b> A Principal Building that incorporates between three (3) and eight (8) side-by-side and/or horizontally stacked Dwelling Units, typically with one or more shared entries, that has the appearance of one (1) House from the Thoroughfare.</p>	<p>CD-4 SD-AE, as indicated in Table 10-I.4.2.1</p>
 <p><b>Live/Work Building</b> A small- to medium-sized attached or detached Structure that consists of a flexible space used for artisan, studio, service, or Commercial Uses, and a residential unit above and/or behind. Both the flex space and the Residential unit are internally connected. This type is appropriate for providing affordable and flexible Mixed Use space for incubating neighborhood-serving retail and service Uses, artists and other craftspeople.</p>	<p>CD-3* CD-4 SD-AE, as indicated in Table 10-I.4.2.1</p>

\*Only if Neighborhood Corner Retail/Service Special Requirement is applicable.

# SECTION 10-I.4: STANDARDS & REQUIREMENTS

TABLE 10-I.4.5.7.1 BUILDING TYPES – SUMMARY

Building Type		Permitted Districts
	<b>Commercial Building</b> A small- to medium-sized attached or detached non-Residential Building, typically designed to facilitate pedestrian-oriented retail, Service Business, Artisan Establishment, and Office Uses.	<b>CD-4</b> <b>SD-AE</b> , as indicated in Table 10-I.4.2.1
	<b>Large Commercial Building</b> A large attached or detached non-Residential Building, typically designed to facilitate Retail, Service Business, Artisan Establishment, and Office Uses.	<b>CD-4**</b> <b>SD-AE</b> , as indicated in Table 10-I.4.2.1
	<b>Mixed-Use Building</b> A typically attached Building that provides a vertical and/or horizontal mix of Uses typically designed to facilitate pedestrian-oriented retail, Lodging, Service Business, or Office Uses on the ground floor, with upper floors typically designed for Residential or Office Uses.	<b>CD-4</b> <b>SD-AE</b> , as indicated in Table 10-I.4.2.1
	<b>Flex Building</b> A large footprint Building designed to accommodate a single Use or a vertical or horizontal mix of Uses.	<b>CD-4**</b> <b>SD-AE</b> , as indicated in Table 10-I.4.2.1
	<b>Civic Building</b> A medium- to large-sized attached or detached Building, dedicated to Civic activities, and is designed to stand apart from its surroundings due to the specialized nature of its Civic purpose. Examples include libraries, Schools, performing arts, and museums. They are often the most prominently sited and architecturally significant structures in a community.  The design and form of Civic Buildings shall be determined after consultation with the Planning Department. If the Building is a public Building, after such consultation, the design and form of the Building shall be subject to approval of the applicable Decision-Making Authority.	<b>CD-1</b> <b>CD-3</b> <b>CD-4</b> <b>SD-AE</b>

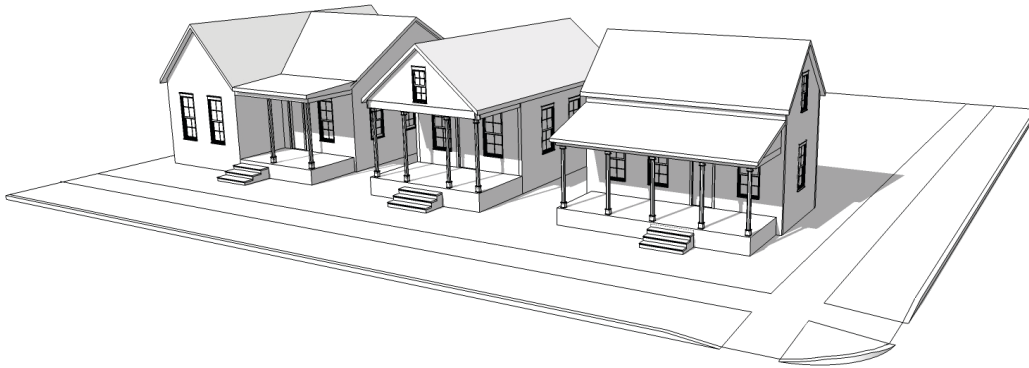
Illustrations are provided for illustrative purposes only.

\*\*Permitted in Route 1 Special Requirement area only

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

### TINY HOME

**TABLE 10-I.4.5.7.2 BUILDING TYPES  
SPECIFIC STANDARDS**



*Illustration provided for illustrative purposes only.*

### Permitted Districts

**CD-3** **CD-4** **SD-AE**, as indicated in Table 10-I.4.2.1

#### General Description

A detached Building not exceeding 400 square feet interior usable area, having an Edgeyard or Sideyard yard type that may be shared with an Accessory Building in the backyard. This Building type typically incorporates a single Residential unit.

#### Lot / Building Site

**Width** Per Character District Lot / Building Site Width

#### Number of Units

**Units per Building** 1 max.

#### Allowed Private Frontages

Common Yard	P
Fence	P
Porch	P
Stoop/Stoopfront	P

#### Building Size and Massing

##### Height

2 Stories max

##### Width

Not Regulated

#### Pedestrian Access

**Main Entrance Location** Principal Frontage

#### Vehicle Access and Parking

Parking may be accessed from an Alley, side street or front street.

#### LEGEND

The following notations are utilized in this table.

**P** Permitted

# SECTION 10-I.4: STANDARDS & REQUIREMENTS

HOUSE

TABLE 10-I.4.5.7.2 BUILDING TYPES  
SPECIFIC STANDARDS

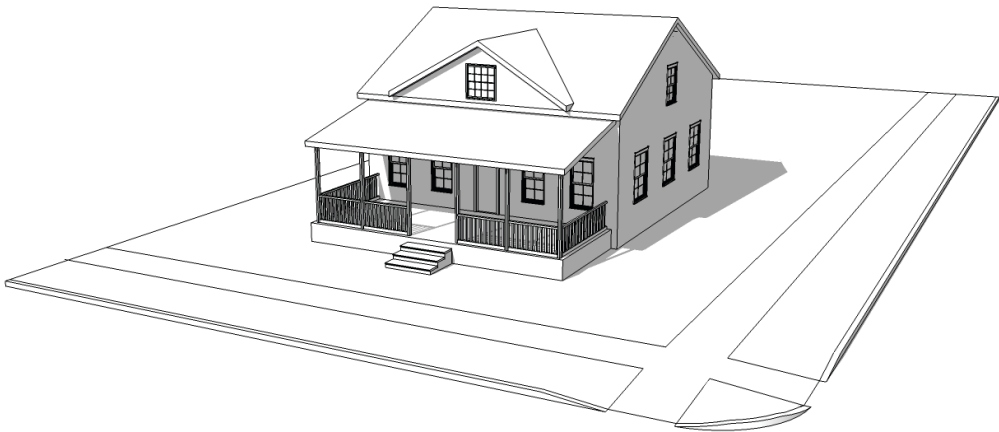


Illustration provided for illustrative purposes only.

### Permitted Districts

CD-3

CD-4

SD-AE, as indicated in Table 10-I.4.2.1

General Description	
A detached Building of more than 400 square feet of interior usable area, having an Edgeyard or Sideyard yard type that may be shared with an Accessory Building in the backyard. This Building type typically incorporates a single Residential unit, but may include a maximum of two (2) units per Building.	
Lot / Building Site	
Width	Per District Lot / Building Site Width
Number of Units	
Units per Building	2 max.
Allowed Private Frontages	
Common Yard	P
Fence	P
Porch	P
Stoop/Stoopfront	P

Building Size and Massing	
Height	2 Stories max
Width	Not Regulated
Pedestrian Access	
Main Entrance Location	Principal Frontage
Vehicle Access and Parking	
Parking may be accessed from an Alley, side street or front street.	

LEGEND

The following notations are utilized in this table.

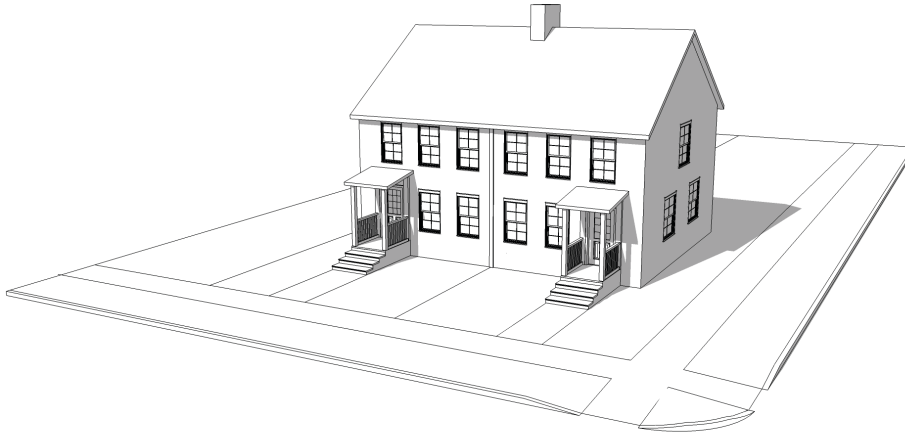
P Permitted



## SECTION 10-I.4: STANDARDS & REQUIREMENTS

### DUPLEX

**TABLE 10-I.4.5.7.2 BUILDING TYPES  
SPECIFIC STANDARDS**



*Illustration provided for illustrative purposes only.*

### Permitted Districts

**CD-3 | CD-4 | SD-AE, as indicated in Table 10-I.4.2.1**

#### General Description

A small- to medium-sized Building with separate entrances at least one of which faces the right-of-way. Both of the units, side-by-side, front-to-back or over-and-under, are contained within a single Building massing. This Building type sits on a small-to-medium sized urban Lot or Building Site having an Edgeyard or Sideyard yard type that may be shared with an Accessory Building in the backyard. This Building type typically incorporates two Residential units, but may include one or more additional Residential units as allowed or required by the Town Zoning Ordinance per Maine LD 2003.

#### Lot / Building Site

**Width** Per District Lot / Building Site Width

#### Number of Units

**Units / Building** 1 per each of 2 attached Buildings max., except as allowed or required by the Town Zoning Ordinance per Maine LD 2003.

#### Allowed Private Frontages

Common Yard	P
Fence	P
Porch	P
Stoop/Stoopfront	P

#### Building Size and Massing

##### Height

2 Stories max

##### Width

48 ft. max.

#### Pedestrian Access

**Main Entrance Location** Principal Frontage

At least one unit must have an individual entry at the Principal Frontage

#### LEGEND

The following notations are utilized in this table.

**P** Permitted

# SECTION 10-I.4: STANDARDS & REQUIREMENTS

TOWNHOUSE

TABLE 10-I.4.5.7.2 BUILDING TYPES  
SPECIFIC STANDARDS



Illustration provided for illustrative purposes only.

## Permitted Districts

CD-4

SD-AE, as indicated in Table 10-I.4.2.1

General Description	
A collection of very narrow to medium sized attached Buildings with a Rearyard yard type (or Sideyard as an end unit) that consists of side-by-side units with individual entries facing the right-of-way. This type shares at least one party wall with another Building of the same type and occupies the full Frontage Line.	
Lot / Building Site	
Width	Per District Lot / Building Site Width
Number of Units	
Units per Townhouse	1 max., except as allowed or required by the Town Zoning Ordinance per Maine LD 2003.
Number of Townhouses connected to other Townhouses	3 min., 8 max.

Allowed Private Frontages	
Porch	P
Stoop/Stoopfront	P
Terrace/Lightwell	P
Dooryard	P
Building Size and Massing	
Height	Per Character District Height Standards
Width	12 ft. min. – 36 ft. max. per Townhouse
Pedestrian Access	
Main Entrance Location	Principal Frontage

LEGEND

The following notations are utilized in this table.

P

 Permitted

# SECTION 10-I.4: STANDARDS & REQUIREMENTS

## SMALL MULTI-FAMILY

TABLE 10-I.4.5.7.2 BUILDING TYPES  
SPECIFIC STANDARDS



Illustration provided for illustrative purposes only.

### Permitted Districts

**CD-4** **SD-AE**, as indicated in Table 10-I.4.2.1

#### General Description

A Principal Building that incorporates between three (3) and eight (8) side-by-side and/or horizontally stacked Dwelling Units, typically with one or more shared entries, that has the appearance of one (1) House from the Thoroughfare.

#### Lot / Building Site

**Width** Per District Lot / Building Site Width

#### Number of Units

**Units per Building** 3 min., 8 max., except as allowed or required by the Town Zoning Ordinance per Maine LD 2003.

#### Allowed Private Frontages

Porch	P
Stoop/Stoopfront	P
Forecourt	P
Terrace/Lightwell	P

#### Building Size and Massing

##### Height

Per District Height Standards

##### Width

Per District Lot / Building Site Width and Side Setback Standards

#### Pedestrian Access

**Main Entrance Location** Principal Frontage

#### LEGEND

The following notations are utilized in this table.

**P** Permitted

# SECTION 10-I.4: STANDARDS & REQUIREMENTS

## LIVE/WORK

TABLE 10-I.4.5.7.2 BUILDING TYPES  
SPECIFIC STANDARDS



Illustration provided for illustrative purposes only.

### Permitted Districts

**CD-3\*** **CD-4** **SD-AE**, as indicated in Table 10-I.4.2.1

#### General Description

A small- to medium-sized attached or detached Structure that consists of a flexible space used for artisan, studio, service, or Commercial Uses, and a residential unit above and/or behind. Both the flex space and the Residential unit are internally connected. This type is appropriate for providing affordable and flexible Mixed Use space for incubating neighborhood-serving retail and service Uses, artists and other craftspeople.

\*Only if Neighborhood Corner Retail/Service Special Requirement is applicable.

#### Lot / Building Site

Width Per District Lot / Building Site Width

#### Number of Units

Units per Building 4 Residential units per 1 flexible space.

#### Allowed Private Frontages

Stoop/Stoopfront	P
Terrace/Lightwell	P
Shopfront	P
Commercialfront	P

#### Building Size and Massing

Height
Per District Height Standards
Width
18 ft min. – 36 ft max.

#### Pedestrian Access

Main Entrance Location	Principal Frontage
------------------------	--------------------

LEGEND

The following notations are utilized in this table.

P

 Permitted

SECTION 10-I.4: STANDARDS & REQUIREMENTS

COMMERCIAL

TABLE 10-I.4.5.7.2 BUILDING TYPES  
SPECIFIC STANDARDS

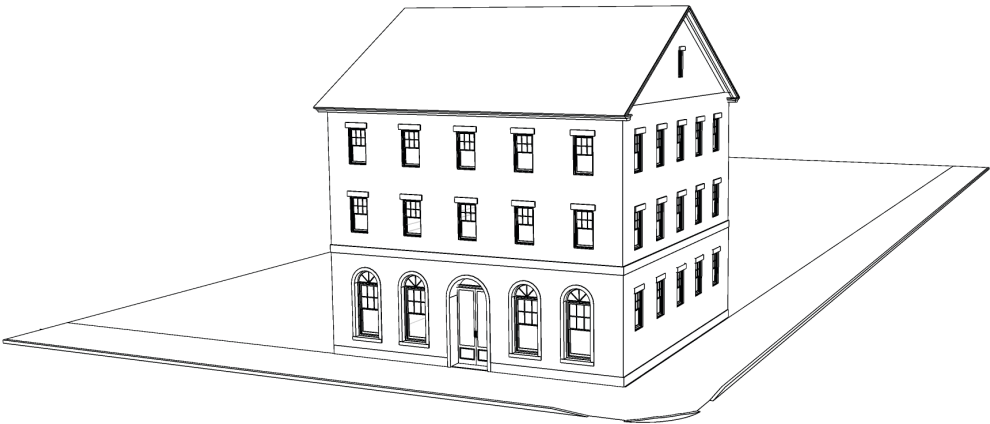


Illustration provided for illustrative purposes only.

Permitted Districts

CD-4 SD-AE, as indicated in Table 10-I.4.2.1

General Description	
A small- to medium-sized attached or detached non-Residential Building, typically designed to facilitate pedestrian-oriented retail, Service Business, Artisan Establishment, and Office Uses.	
Lot / Building Site	
Width	Per District Lot / Building Site Width
Number of Units	
Units per Building	Unrestricted, except as otherwise provided in the Building Code

Allowed Private Frontages	
Stoop/Stoopfront	P
Forecourt	P
Terrace/Lightwell	P
Commercialfront	P
Building Size and Massing	
Height	
Per District Height Standards	
Width	
Per District Lot / Building Site Width and Side Setback Standards	
Pedestrian Access	
Main Entrance Location	Principal Frontage

LEGEND

The following notations are utilized in this table.

P

 Permitted

# SECTION 10-I.4: STANDARDS & REQUIREMENTS

LARGE COMMERCIAL

TABLE 10-I.4.5.7.2 BUILDING TYPES  
SPECIFIC STANDARDS

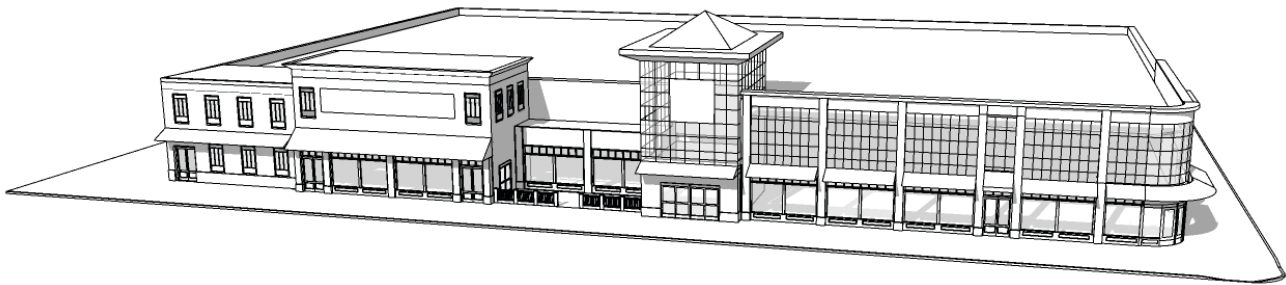


Illustration provided for illustrative purposes only.

## Permitted Districts

CD-4\*

SD-AE, as indicated in Table 10-I.4.2.1

General Description			
A large attached or detached non-Residential Building, typically designed to facilitate Retail, Service Business, Artisan Establishment, and Office Uses.			
*Permitted in Route 1 Special Requirement area only			
Lot / Building Site			
Width	Per District Lot / Building Site Width		
Number of Units			
Units per Building	Not Regulated		
Allowed Private Frontages			
Stoop/Stoopfront	P	Terrace	P
Forecourt	P	Commercialfront	P
Terrace/Lightwell	P	Gallery**	P
Shopfront	P	Arcade**	P
**May only be used in conjunction with a Shopfront or Commercialfront Frontage type.			

Building Size and Massing	
Height	Per District Building Height Standards
Width	Per District Lot / Building Site Width and Side Setback Standards
Pedestrian Access	
Ground Floor Units	Principal Frontage
Upper Floors Units	Principal or Secondary Frontage
Upper floors units must be accessed by a common entry	

LEGEND

The following notations are utilized in this table.

P Permitted



## SECTION 10-I.4: STANDARDS & REQUIREMENTS

### MIXED USE

**TABLE 10-I.4.5.7.2 BUILDING TYPES  
SPECIFIC STANDARDS**



*Illustration provided for illustrative purposes only.*

### Permitted Districts

**CD-4** **SD-AE**, as indicated in Table 10-I.4.2.1

#### General Description

A typically attached Building that provides a vertical and/or horizontal mix of Uses typically designed to facilitate pedestrian-oriented retail, Lodging, Service Business, or Office Uses on the ground floor, with upper floors typically designed for Residential or Office Uses.

#### Lot / Building Site

**Width** Per District Lot / Building Site Width

#### Number of Units

**Units per Building** 2 min., which must accommodate different Principal Uses

#### Allowed Private Frontages

<b>Stoop/Stoopfront</b>	<b>P</b>	<b>Commercialfront</b>	<b>P</b>
<b>Terrace/Lightwell</b>	<b>P</b>	<b>Gallery*</b>	<b>P</b>

#### Allowed Private Frontages (continued)

<b>Shopfront</b>	<b>P</b>	<b>Arcade*</b>	<b>P</b>
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\*May only be used in conjunction with a Shopfront or Commercialfront Frontage type.

#### Building Size and Massing

##### Height

2 Stories min; 4 Stories max in CD-4

##### Width

Per District Lot / Building Site Width and Side Setback Standards

#### Pedestrian Access

**Ground Floor Units** Principal Frontage

**Upper Floors Units** Principal or Secondary Frontage

Upper floors units must be accessed by a common entry

#### LEGEND

The following notations are utilized in this table.

**P** Permitted

# SECTION 10-I.4: STANDARDS & REQUIREMENTS

FLEX

TABLE 10-I.4.5.7.2 BUILDING TYPES  
SPECIFIC STANDARDS

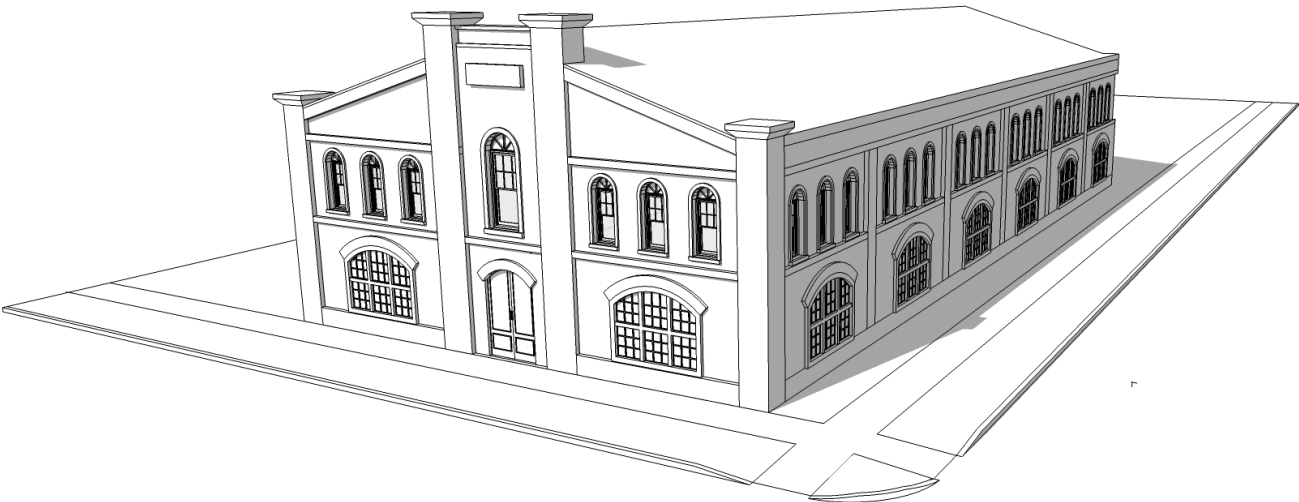


Illustration provided for illustrative purposes only.

## Permitted Districts

**CD-4\*** | **SD-AE**, as indicated in Table 10-I.4.2.1

### General Description

A large footprint Building designed to accommodate a single Use or a vertical or horizontal mix of Uses.

\*Permitted in Route 1 Special Requirement area only

### Lot / Building Site

Width Per District Lot / Building Site Width

### Number of Units

Not Regulated

### Allowed Private Frontages

Stoop/Stoopfront	P	Commercialfront	P
Forecourt	P	Gallery**	P
Terrace/Lightwell	P	Arcade**	P
Shopfront	P		

\*\*May only be used in conjunction with a Shopfront or Commercialfront Frontage type.

### Building Size and Massing

#### Height

Minimum as prescribed for applicable Character District; 3 stories max.

#### Width

Per District Lot / Building Site Width and Side Setback Standards

### Pedestrian Access

Main Entrance Location Principal Frontage

LEGEND

The following notations are utilized in this table.

P Permitted

SECTION 10-I.4: STANDARDS & REQUIREMENTS

CIVIC

TABLE 10-I.4.5.7.2 BUILDING TYPES  
SPECIFIC STANDARDS

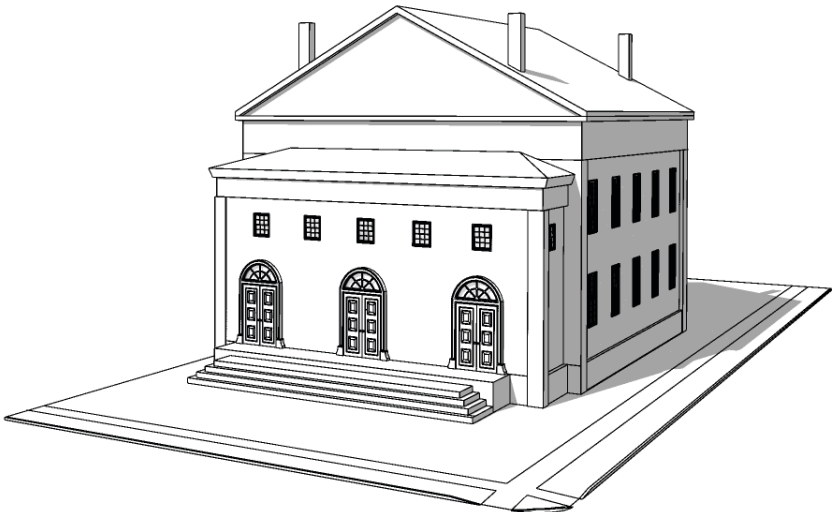


Illustration provided for illustrative purposes only.

Permitted Districts

CD-1 CD-3 CD-4 SD-AE, as indicated in Table 10-I.4.2.1

General Description

A medium- to large-sized attached or detached Principal Building, dedicated to Civic activities, and is designed to stand apart from its surroundings due to the specialized nature of its Civic purpose. Examples include libraries, Schools, performing arts, and museums. They are often the most prominently sited and architecturally significant structures in a community.

The design and form of Civic Buildings shall be determined after consultation with the Planning Department. If the Building is a public Building, after such consultation, the design and form of the Building shall be subject to approval of the applicable Decision-Making Authority.

Lot / Building Site

Width Not Regulated

Number of Units

Not Applicable

Building Size and Massing

Height

The height of Civic Buildings shall be limited to 20 additional feet above the maximum allowed height in any Adjacent District.

Width

Not Regulated

Pedestrian Access

Except as specifically provided in this Section, Civic Buildings are not subject to the Building, Lot or Building Site, or Private Frontage requirements of this Section.

LEGEND

The following notations are utilized in this table.



Permitted

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

### SECTION 10-I.4.6

#### ENCROACHMENTS & EASEMENTS.

##### 10-I.4.6.1 Encroachments.

Encroachments of Building Elements into a Setback shall be allowed within each District as set forth in **Table 10-I.4.2.1 (District & Civic Space Standards)**.

##### 10-I.4.6.2 Easements.

1. Each new Lot or Building Site without public sewer shall provide an Easement for primary and secondary septic fields.
2. Each new Lot or Building Site without public water shall provide an Easement for well radius outside of the Lot or Building Site.

### SECTION 10-I.4.7 USES.

##### 10-I.4.7.1 Principal Uses.

1. Except as otherwise specified for Civic Spaces, Buildings, Lots, and Building Sites in each District shall conform to the Principal Uses specified for the applicable District on **Table 10-I.4.7.1A (Permitted Principal Uses)**. Multiple Principal Uses may exist within a Building, Structure, Lot, or Building Site. Within Civic Spaces, Buildings, Lots, and Building Sites shall conform to the Principal Uses specified for Civic Spaces (CS) on **Table 10-I.4.7.1A (Permitted Principal Uses)**, irrespective of Uses specified for the District in which such Civic Spaces are located.
2. Principal Uses are Permitted as of Right ("P"), Permitted by Special Use Permit ("PS"), Not Permitted ("NP"), or Not Regulated ("NR") within each District or Civic Space, in each case as indicated in **Table 10-I.4.7.1A (Permitted Principal Uses)**.
3. Principal Uses that are indicated in **Table 10-I.4.7.1A (Permitted Principal Uses)** as Permitted as of Right ("P") or Permitted by Special Use Permit

("PS") are permitted within each District or Civic Space, subject to compliance with all applicable standards, conditions and requirements thereof and obtaining all applicable approvals and permits therefor.

4. Residential Principal Uses within any Development Site of five (5) or more acres of Net Developable Site Area shall be limited to 70% of the Net Developable Site Area.

##### 10-I.4.7.2 Prohibited Uses.

Any Use that is not specifically listed for a District or Civic Space, as applicable, as Permitted as of Right ("P") or Permitted by Special Use Permit ("PS"), and any Use that is specifically listed as Not Permitted ("NP") in such **Table 10-I.4.7.1A (Permitted Principal Uses)** is prohibited.

##### 10-I.4.7.3 Accessory Uses.

1. Except as otherwise specified for Civic Spaces, Buildings, Lots, and Building Sites in each District shall conform to the Accessory Uses specified on **Table 10-I.4.7.1B (Permitted Accessory Uses)** for such District. Within Civic Spaces, Buildings, Lots, and Building Sites shall conform to the Principal Uses specified for Civic Spaces (CS) on **Table 10-I.4.7.1B (Permitted Accessory Uses)**, irrespective of Uses specified for the District in which such Civic Spaces are located.
2. Accessory Uses that are subordinate and incidental to a permitted Principal Use and are indicated in **Table 10-I.4.7.1B (Permitted Accessory Uses)** for the applicable District as Permitted as of Right ("P") or Permitted by Special Use Permit ("PS") are permitted to the extent indicated in **Table 10-I.4.7.1B (Permitted Accessory Uses)**, subject to compliance with all applicable standards, conditions and requirements thereof and obtaining all applicable approvals and permits therefore.
3. Any Accessory Use that is indicated as Not Permitted ("NP") in such Table is prohibited.

### 10-I.4.7.4 Temporary Uses.

1. Except as otherwise specified for Civic Spaces, Buildings, Lots, and Building Sites in each District shall conform to the Temporary Uses specified on **Table 10-I.4.7.1C (Permitted Temporary Uses)** for such District. Within Civic Spaces, Buildings, Lots, and Building Sites shall conform to the Principal Uses specified for Civic Spaces (CS) on **Table 10-I.4.7.1B (Permitted Accessory Uses)**, irrespective of Uses specified for the District in which such Civic Spaces are located.
2. Temporary Uses that are indicated in **Table 10-I.4.7.1C (Permitted Temporary Uses)** as Permitted as of Right ("P") or Permitted by Special Use Permit ("PS") are permitted to the extent indicated in **Table 10-I.4.7.1C (Permitted Temporary Uses)**.
3. Any Temporary Use that is indicated as Not Permitted ("NP") in such Table is prohibited.

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**TABLE 10-I.4.7.1A PERMITTED PRINCIPAL USES**

PRINCIPAL USE	CD-1	CD-3	CD-4	SD-AE	CS
<b>RESIDENTIAL</b>					
Elderly Congregate Housing	NP	NP	PS	NP	NP
Elderly Housing	NP	P	P	P *	NP
Residential Portion of Live/Work <sup>1</sup> or Mixed Use Building	NP	P <sup>1</sup>	P	P *	NP
Single-Family Detached Residential Dwelling in House or Tiny Home	NP	P	P	P *	NP
Single-Family Attached Residential Dwelling in Townhouse	NP	NP	P	P *	NP
Multi-Family Attached Residential in Small Multi-family Building	NP	NP	P	P *	NP
Two-Family Attached Residential Dwellings in Duplex	NP	P	P	P *	NP
Other Residential Use not listed under any Use category	NP	NP	NP	NP	NP
<b>COMMERCIAL</b>					
Animal Boarding	NP	NP	NP	NP	NP
Animal Hospital/Veterinary Clinic	NP	NP	P	P	NP
Artisanal Food and/or Beverage Facility	NP	P <sup>1</sup>	P	P *	NP
Artisan Establishment	NP	NP	P	P *	NP
Banks	NP	NP	P	P *	NP
Boarding House	NP	PS	PS	PS	NP
Brew Pub	NP	NP	P	P *	NP
Commercial Schools	NP	NP	P	P *	NP

<b>LEGEND</b> The following notations are utilized in this table.	
	Permitted as of Right
	Permitted by Special Use Permit
	Not Permitted
	Not Regulated
	Permitted, except in A & E Area, as determined by applicable Decision-Making Authority
	Not Permitted, except in A & E Area, as determined by applicable Decision-Making Authority
	Permitted only with Special Use Permit & if Corner Parking Lot (Commercial) Special Requirement is applicable

<b>FOOTNOTES</b>
<sup>1</sup> Only if Neighborhood Corner Retail/Service Special Requirement is applicable.
<sup>2</sup> West of Route 1.
<sup>3</sup> Public Transportation Station is Not Permitted in CD-3.
<sup>4</sup> Does not count towards Civic Space Percentage Requirement noted in Section 10-I.3.2.2.
<sup>5</sup> Only if Light Industrial Special Requirement is applicable.
<sup>6</sup> Gasoline pumps and any service doors must not Enfront Route 1.
<sup>7</sup> Only if Route 1 Special Requirement is applicable. Associated garages, Parking Areas, and service bays permitted in 3rd Layer only.
<sup>8</sup> Permit not required for telecommuting.



## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**TABLE 10-I.4.7.1A PERMITTED PRINCIPAL USES**

PRINCIPAL USE	CD-1	CD-3	CD-4	SD-AE	CS
Day Care Center (Commercial)	NP	NP	P	P *	NP
Display Gallery (Commercial)	NP	NP	P	P *	NP
Farm Stands	NP	P <sup>1</sup>	P	P *	P
Formula Restaurant or Fast-Food Restaurant	NP	NP	NP	NP	NP
Greenhouse	NP	NP	NP	NP	NP
Hospitals (Commercial)	NP	NP	NP	NP	NP
Hotel – (other than Full-Service)	NP	NP	NP	P *	NP
Hotel – Full-Service	NP	NP	NP	P *	NP
Ice Cream Stand	NP	PS	P	P *	NP
Landscape Business	NP	NP	NP	P *	NP
Lodging Home / Tourist Home / Inn	NP	P	P	P *	NP
Marina (Commercial)	NP	NP	P	P *	P
Medical Facility (Commercial)	NP	NP	PS	NP	NP
Mini-Storage	NP	NP	NP	NP	NP
Mortuary, Undertaking or Funeral Establishment	NP	NP	PS	NP	NP
Motel, in 1st or 2nd Layer	NP	NP	NP	NP	NP
Motel, in 3rd Layer	NP	NP	NP	PS	NP
Movie Theater (Commercial)	NP	NP	P	P *	NP

<b>LEGEND</b> The following notations are utilized in this table.	
	Permitted as of Right
	Permitted by Special Use Permit
	Not Permitted
	Not Regulated
	Permitted, except in A & E Area, as determined by applicable Decision-Making Authority
	Not Permitted, except in A & E Area, as determined by applicable Decision-Making Authority
	Permitted only with Special Use Permit & if Corner Parking Lot (Commercial) Special Requirement is applicable

<b>FOOTNOTES</b>
<sup>1</sup> Only if Neighborhood Corner Retail/Service Special Requirement is applicable.
<sup>2</sup> West of Route 1.
<sup>3</sup> Public Transportation Station is Not Permitted in CD-3.
<sup>4</sup> Does not count towards Civic Space Percentage Requirement noted in Section 10-I.3.2.2.
<sup>5</sup> Only if Light Industrial Special Requirement is applicable.
<sup>6</sup> Gasoline pumps and any service doors must not Enfront Route 1.
<sup>7</sup> Only if Route 1 Special Requirement is applicable. Associated garages, Parking Areas, and service bays permitted in 3rd Layer only.
<sup>8</sup> Permit not required for telecommuting.

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**TABLE 10-I.4.7.1A PERMITTED PRINCIPAL USES**

PRINCIPAL USE	CD-1	CD-3	CD-4	SD-AE	CS
Museum (Commercial)	NP	NP	P	P *	P
Nursing Homes (Commercial)	NP	NP	PS	NP	NP
Obscene Exhibitions	NP	NP	NP	NP	NP
Open Market (Commercial)	NP	NP	P	P *	NP
Parking Lot (Commercial)	NP	PS **	PS	PS	NP
Parking Structure (Commercial)	NP	NP	PS	PS	NP
Other Restaurants not otherwise specifically listed	NP	P <sup>1</sup>	P	P *	NP
Other Retail Sales not otherwise specifically listed	NP	NP	P	P *	NP
Service Business	NP	P <sup>1</sup>	P	P *	NP
Short-Term Rental	NP	P	P	P *	NP
Store for Retail Sales of merchandise with fewer than 2,500 square feet of gross floor area	NP	P <sup>1</sup>	P	P	NP
Store for Retail Sales of merchandise with 2,500–15,000 square feet of gross floor area	NP	NP	PS <sup>2</sup>	NP	NP
Other Commercial Use not listed under any Use category	NP	NP	NP	NP	NP
<b>OFFICE</b>					
Laboratory or Research Facility	NP	NP	P	P *	NP
Offices and Clinics for Medical, Psychiatric, or Other Health Services for the Examination or Treatment of Persons as Outpatients	NP	NP	P	P *	NP

<b>LEGEND</b> The following notations are utilized in this table.	
	Permitted as of Right
	Permitted by Special Use Permit
	Not Permitted
	Not Regulated
	Permitted, except in A & E Area, as determined by applicable Decision-Making Authority
	Not Permitted, except in A & E Area, as determined by applicable Decision-Making Authority
	Permitted only with Special Use Permit & if Corner Parking Lot (Commercial) Special Requirement is applicable

<b>FOOTNOTES</b>
<sup>1</sup> Only if Neighborhood Corner Retail/Service Special Requirement is applicable.
<sup>2</sup> West of Route 1.
<sup>3</sup> Public Transportation Station is Not Permitted in CD-3.
<sup>4</sup> Does not count towards Civic Space Percentage Requirement noted in Section 10-I.3.2.2.
<sup>5</sup> Only if Light Industrial Special Requirement is applicable.
<sup>6</sup> Gasoline pumps and any service doors must not Enfront Route 1.
<sup>7</sup> Only if Route 1 Special Requirement is applicable. Associated garages, Parking Areas, and service bays permitted in 3rd Layer only.
<sup>8</sup> Permit not required for telecommuting.

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**TABLE 10-I.4.7.1A PERMITTED PRINCIPAL USES**

PRINCIPAL USE	CD-1	CD-3	CD-4	SD-AE	CS
Radio or Television Studio	NP	NP	P	P *	NP
Town of York or York School District Offices	NP	P	P	P *	P
Other Office Use not listed under any Use category	NP	NP	P	P *	NP
<b>CIVIC &amp; PUBLIC</b>					
Bus Shelter or Public Transportation Station	NP	P <sup>3</sup>	P	P *	P
Cemeteries	NP	PS	NP	NP	P
Civic Space Type – Community Garden	NP	P	P	P *	P
Botanic Garden	P	NP	P	NP	P
Civic Space Type – Green	NP	P	P	P *	P
Civic Space Type – Natural Area	P	P <sup>4</sup>	P <sup>4</sup>	P *	P
Civic Space Type – Playground	P	P	P	P *	P
Civic Space Type – Plaza	NP	NP	P	P *	P
Civic Space Type – Pocket Park	P	P	P	P *	P
Civic Space Type – Sports Field or Court	NP	P	P	P *	P
Civic Space Type – Square	NP	P	P	P *	P
Conference Center or Convention Center	NP	NP	NP	NP	NP

**LEGEND** The following notations are utilized in this table.

<b>P</b>	Permitted as of Right
<b>PS</b>	Permitted by Special Use Permit
<b>NP</b>	Not Permitted
<b>NR</b>	Not Regulated
<b>P</b> *	Permitted, except in A & E Area, as determined by applicable Decision-Making Authority
<b>NP</b> *	Not Permitted, except in A & E Area, as determined by applicable Decision-Making Authority
<b>PS</b> **	Permitted only with Special Use Permit & if Corner Parking Lot (Commercial) Special Requirement is applicable

### FOOTNOTES

<sup>1</sup> Only if Neighborhood Corner Retail/Service Special Requirement is applicable.
<sup>2</sup> West of Route 1.
<sup>3</sup> Public Transportation Station is Not Permitted in CD-3.
<sup>4</sup> Does not count towards Civic Space Percentage Requirement noted in Section 10-I.3.2.2.
<sup>5</sup> Only if Light Industrial Special Requirement is applicable.
<sup>6</sup> Gasoline pumps and any service doors must not Enfront Route 1.
<sup>7</sup> Only if Route 1 Special Requirement is applicable. Associated garages, Parking Areas, and service bays permitted in 3rd Layer only.
<sup>8</sup> Permit not required for telecommuting.

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**TABLE 10-I.4.7.1A PERMITTED PRINCIPAL USES**

PRINCIPAL USE	CD-1	CD-3	CD-4	SD-AE	CS
Cultural Facility	NP	NP	P	P *	P
Day Care Center (Non-Commercial)	NP	NP	P	P *	NP
Display Gallery (Non-Commercial)	NP	NP	P	P *	P
Essential Services	NP	PS	PS <sup>2</sup> P	P *	P
Exhibition Center (Non-Commercial)	NP	NP	NP	NP	NP
Fire Station	NP	P	P	P *	P
Fountain	NP	P	P	P	P
Hospitals (Non-Commercial)	NP	NP	NP	NP	NP
Library	NP	NP	P	P *	P
Marina (Non-Commercial)	NP	NP	PS	NP	PS
Live Theater	NP	NP	P	P *	P
Medical Facility (Non-Commercial)	NP	NP	PS	NP	NP
Membership Organization organized for Civic Purposes only (See also Membership Organizations [non-Civic] under "Recreation & Amusement" Use category)	NP	NP	P	P *	P
Movie Theater (Non-Commercial)	NP	NP	P	P *	NP
Municipal Use	NP	PS	PS	PS	PS
Museum (Non-Commercial)	NP	NP	P	P *	P
Nursing Homes (Non-Commercial)	NP	NP	PS	NP	NP

<b>LEGEND</b> The following notations are utilized in this table.	
	Permitted as of Right
	Permitted by Special Use Permit
	Not Permitted
	Not Regulated
	Permitted, except in A & E Area, as determined by applicable Decision-Making Authority
	Not Permitted, except in A & E Area, as determined by applicable Decision-Making Authority
	Permitted only with Special Use Permit & if Corner Parking Lot (Commercial) Special Requirement is applicable

<b>FOOTNOTES</b>
<sup>1</sup> Only if Neighborhood Corner Retail/Service Special Requirement is applicable.
<sup>2</sup> West of Route 1.
<sup>3</sup> Public Transportation Station is Not Permitted in CD-3.
<sup>4</sup> Does not count towards Civic Space Percentage Requirement noted in Section 10-I.3.2.2.
<sup>5</sup> Only if Light Industrial Special Requirement is applicable.
<sup>6</sup> Gasoline pumps and any service doors must not Enfront Route 1.
<sup>7</sup> Only if Route 1 Special Requirement is applicable. Associated garages, Parking Areas, and service bays permitted in 3rd Layer only.
<sup>8</sup> Permit not required for telecommuting.

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**TABLE 10-I.4.7.1A PERMITTED PRINCIPAL USES**

PRINCIPAL USE	CD-1	CD-3	CD-4	SD-AE	CS
Open Market (Non-Commercial)	NP	NP	P	P *	PS
Parking Lot (Non-Commercial)	NP	PS	PS	PS	PS
Parking Structure (Non-Commercial)	NP	NP	PS	PS	PS
Police Station	NP	P	P	NP	P
Public Art	PS	PS	PS	PS	PS
Public Assembly	NP	P	P	P	P
Religious Use	NP	P	P	P	P
School (Non-Commercial)	NP	P	P	NP	P
Utility District	NP	PS	PS	NP	PS
Other Civic & Public Use not listed under any Use category	PS	P	P	P *	P
<b>INDUSTRIAL</b>					
Heavy Industrial	NP	NP	NP	NP	NP
Light Industrial, excluding Artisan Establishments	NP	P <sup>5</sup>	NP	NP	NP
Other Industrial Use not listed under any Use category	NP	NP	NP	NP	NP
<b>VEHICULAR</b>					
Gasoline Station <sup>6</sup>	NP	NP	PS <sup>2</sup>	NP	NP
Place for Repair, Sale, Rent or Storage of Pleasure Boats	NP	NP	NP	NP	NP
Public Transportation Station (See "Civic & Public" Use category)	--	--	--	--	--
Sale of Pickup Coaches/Campers, Tent Trailers and Similar Equipment Including Snowmobile	NP	NP	NP	NP	NP
Sale, Rental and Accessory Storage of Automobiles, Light Trucks, Motorcycles, and Mopeds Conducted Wholly or Partially in Open Lots or Building Sites	NP	NP	NP	NP	NP

**LEGEND** The following notations are utilized in this table.

<b>P</b>	Permitted as of Right
<b>PS</b>	Permitted by Special Use Permit
<b>NP</b>	Not Permitted
<b>NR</b>	Not Regulated
<b>P</b> *	Permitted, except in A & E Area, as determined by applicable Decision-Making Authority
<b>NP</b> *	Not Permitted, except in A & E Area, as determined by applicable Decision-Making Authority
<b>PS</b> **	Permitted only with Special Use Permit & if Corner Parking Lot (Commercial) Special Requirement is applicable

### FOOTNOTES

<sup>1</sup> Only if Neighborhood Corner Retail/Service Special Requirement is applicable.
<sup>2</sup> West of Route 1.
<sup>3</sup> Public Transportation Station is Not Permitted in CD-3.
<sup>4</sup> Does not count towards Civic Space Percentage Requirement noted in Section 10-I.3.2.2.
<sup>5</sup> Only if Light Industrial Special Requirement is applicable.
<sup>6</sup> Gasoline pumps and any service doors must not Enfront Route 1.
<sup>7</sup> Only if Route 1 Special Requirement is applicable. Associated garages, Parking Areas, and service bays permitted in 3rd Layer only.
<sup>8</sup> Permit not required for telecommuting.

## SECTION 10–I.4: STANDARDS & REQUIREMENTS

**TABLE 10–I.4.7.1A PERMITTED PRINCIPAL USES**

PRINCIPAL USE	CD-1	CD-3	CD-4	SD-AE	CS
Vehicle Maintenance	NP	NP	P <sup>7</sup>	NP	NP
Vehicle Service	NP	NP	NP	NP	NP
Other Vehicular Use not listed under any Use category	NP	NP	NP	NP	NP
<b>RURAL &amp; AGRICULTURAL</b>					
Animal Breeding	NP	NP	NP	NP	NP
Animal Hospital or Veterinary Clinic	NP	NP	PS	NP	NP
Aquaculture	NP	NP	P	NP	NP
Forest Management Activities Except for Timber Harvesting	P	P	P	P	P
General Purpose Farm, Agriculture, Nursery, or Greenhouse	NP	NP	NP	NP	NP
Harvesting of Wild Crops	NP	P	P	P	P
Kennel	NP	NP	NP	NP	NP
Soil and Water Conservation Practices, per USDA Natural Resources Conservation Service	P	P	P	P	P
Stable	NP	NP	NP	NP	NP
Timber Harvesting, if Screened	P	P	P	P	P
Wildlife Management Practices, excluding hunting	P	P	NP	NP	P
Other Rural & Agricultural Use not listed under any Use category	NP	NP	NP	NP	NP
<b>RECREATION &amp; AMUSEMENT</b>					
Amusement Arcades	NP	NP	PS	P	NP
Athletic Club/Fitness Center	NP	P <sup>1</sup>	P	P <sup>*</sup>	NP
Bath House	NP	NP	NP	NP	P

<b>LEGEND</b> The following notations are utilized in this table.	
	Permitted as of Right
	Permitted by Special Use Permit
	Not Permitted
	Not Regulated
	Permitted, except in A & E Area, as determined by applicable Decision-Making Authority
	Not Permitted, except in A & E Area, as determined by applicable Decision-Making Authority
	Permitted only with Special Use Permit & if Corner Parking Lot (Commercial) Special Requirement is applicable

<b>FOOTNOTES</b>
<sup>1</sup> Only if Neighborhood Corner Retail/Service Special Requirement is applicable.
<sup>2</sup> West of Route 1.
<sup>3</sup> Public Transportation Station is Not Permitted in CD-3.
<sup>4</sup> Does not count towards Civic Space Percentage Requirement noted in Section 10–I.3.2.2.
<sup>5</sup> Only if Light Industrial Special Requirement is applicable.
<sup>6</sup> Gasoline pumps and any service doors must not Enfront Route 1.
<sup>7</sup> Only if Route 1 Special Requirement is applicable. Associated garages, Parking Areas, and service bays permitted in 3rd Layer only.
<sup>*</sup> Permit not required for telecommuting.



## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**TABLE 10-I.4.7.1A PERMITTED PRINCIPAL USES**

PRINCIPAL USE	CD-1	CD-3	CD-4	SD-AE	CS
Campgrounds and Travel Trailer Parks, which legally existed prior to enactment of this Article, and any expansion thereof on a Lot of Record that legally existed prior to enactment of this Article	NP	P	P	NP	NP
Campgrounds and Travel Trailer Parks created after enactment of this Article	NP	NP	NP	NP	NP
Membership Organizations (non-Civic)	NP	NP	P	P*	P
Golf Club, Golf Course, or Country Club	NP	NP	NP	NP	NP
Indoor Amusement/Entertainment/Assembly Place	NP	NP	PS	P	NP
Indoor Sports Facility	NP	NP	P	P*	P
Open Air or Drive-In Theater or Other Open Air Places of Entertainment	NP	NP	PS	NP	NP
Zoo	NP	NP	NP	NP*	NP
Outdoor Sport and Amusement Facilities Conducted for Profit	NP	NP	PS	NP	NP
Other Recreation & Amusement Use not listed under any Use category	NP	NP	NP	NP	NP
<b>MISCELLANEOUS</b>					
Place for Exhibition, Lettering, or Sale of Gravestones	NP	NP	NP	NP	NP
Schools (Non-Commercial)	NP	P	P	P	P
Other Miscellaneous Use not listed under any Use category	NP	NP	NP	NP	NP

<b>LEGEND</b> The following notations are utilized in this table.	
	Permitted as of Right
	Permitted by Special Use Permit
	Not Permitted
	Not Regulated
	Permitted, except in A & E Area, as determined by applicable Decision-Making Authority
	Not Permitted, except in A & E Area, as determined by applicable Decision-Making Authority
	Permitted only with Special Use Permit & if Corner Parking Lot (Commercial) Special Requirement is applicable

<b>FOOTNOTES</b>
<sup>1</sup> Only if Neighborhood Corner Retail/Service Special Requirement is applicable.
<sup>2</sup> West of Route 1.
<sup>3</sup> Public Transportation Station is Not Permitted in CD-3.
<sup>4</sup> Does not count towards Civic Space Percentage Requirement noted in Section 10-I.3.2.2.
<sup>5</sup> Only if Light Industrial Special Requirement is applicable.
<sup>6</sup> Gasoline pumps and any service doors must not Enfront Route 1.
<sup>7</sup> Only if Route 1 Special Requirement is applicable. Associated garages, Parking Areas, and service bays permitted in 3rd Layer only.
<sup>8</sup> Permit not required for telecommuting.

## SECTION 10–I.4: STANDARDS & REQUIREMENTS

**TABLE 10–I.4.7.1B PERMITTED ACCESSORY USES**

ACCESSORY USE	CD-1	CD-3	CD-4	SD-AE	CS
Accessory Dwelling Unit	NP	P	P	P	NP
Animal Boarding – Indoor, as Accessory Use to a permitted Animal Hospital / Veterinary Clinic Principal Use	NP	NP	P	NP	NP
Bed & Breakfast, accessory to Residential Principal Use	NP	PS	PS	PS	NP
Permanent Bulk Storage Collection Bin	NP	NP	P	P*	P
Drive-Through Facilities	NP	NP	NP	NP	NP
Greenhouse, accessory to Residential Principal Use, solely for personal use of resident, without sales of plants or products or other commercial, agricultural, or Home Occupation purposes	NP	PS	NP	NP	NP
Home Occupation <sup>7</sup>	NP	P	P	NP	NP
Kiosk	NP	NP	PS	NP	NP
Piers, Docks, Wharves, Breakwaters, Causeways, Bridges and Other Structures and Uses Extending Over or Below the Normal High Water Mark	PS	PS	PS	PS	PS
Sale of Produce Raised on Same Premises	NP	P	P	P	P
Geothermal System	NP	P	P	NP	P
Other Accessory Use not listed under any Use Category	NP	PS	PS	NP	PS








<b>LEGEND</b> The following notations are utilized in this table.	
	Permitted as of Right
	Permitted by Special Use Permit
	Not Permitted
	Not Regulated
	Permitted, except in A & E Area, as determined by applicable Decision-Making Authority
	Not Permitted, except in A & E Area, as determined by applicable Decision-Making Authority
	Permitted only with Special Use Permit & if Corner Parking Lot (Commercial) Special Requirement is applicable

<b>FOOTNOTES</b>
<sup>1</sup> Only if Neighborhood Corner Retail/Service Special Requirement is applicable.
<sup>2</sup> West of Route 1.
<sup>3</sup> Public Transportation Station is Not Permitted in CD-3.
<sup>4</sup> Does not count towards Civic Space Percentage Requirement noted in Section 10–I.3.2.2.
<sup>5</sup> Only if Light Industrial Special Requirement is applicable.
<sup>6</sup> Gasoline pumps and any service doors must not Enfront Route 1.
<sup>7</sup> Only if Route 1 Special Requirement is applicable. Associated garages, Parking Areas, and service bays permitted in 3rd Layer only.
<sup>8</sup> Permit not required for telecommuting.

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**TABLE 10-I.4.7.1C PERMITTED TEMPORARY USES**

TEMPORARY USE	CD-1	CD-3	CD-4	SD-AE	CS
Filling or Other Earthmoving Activities	NP	PS	PS	PS	PS
Road and Driveway Construction	PS	PS	PS	PS	PS
Other Temporary Uses not listed	NP	PS	PS	PS	PS

<b>LEGEND</b> The following notations are utilized in this table.	
	Permitted as of Right
	Permitted by Special Use Permit
	Not Permitted
	Not Regulated
	Permitted, except in A & E Area, as determined by applicable Decision-Making Authority
	Not Permitted, except in A & E Area, as determined by applicable Decision-Making Authority
	Permitted only with Special Use Permit & if Corner Parking Lot (Commercial) Special Requirement is applicable

<b>FOOTNOTES</b>
<sup>1</sup> Only if Neighborhood Corner Retail/Service Special Requirement is applicable.
<sup>2</sup> West of Route 1.
<sup>3</sup> Public Transportation Station is Not Permitted in CD-3.
<sup>4</sup> Does not count towards Civic Space Percentage Requirement noted in Section 10-I.3.2.2.
<sup>5</sup> Only if Light Industrial Special Requirement is applicable.
<sup>6</sup> Gasoline pumps and any service doors must not Enfront Route 1.
<sup>7</sup> Only if Route 1 Special Requirement is applicable. Associated garages, Parking Areas, and service bays permitted in 3rd Layer only.
<sup>8</sup> Permit not required for telecommuting.

SECTION 10-I.4: STANDARDS & REQUIREMENTS

SECTION 10-I.4.8  
PARKING.

10-I.4.8.1 General.

All Parking Areas, Parking Structures, Parking Lots, Garages, and other Parking accommodations shall be provided, located, designed, accessed, constructed, maintained, operated, and otherwise meet the standards and requirements specified for the applicable District in **Table 10-I.4.2.1 (District & Civic Space Standards)**.

10-I.4.8.2 Parking Spaces Required.

- 1. Vehicular Parking shall be provided in accordance with this Section 10-I.4.8.2 and **Table 10-I.4.8.2 (Vehicular Parking Requirements)**, subject to reduction pursuant to Section 10.I.4.8.3, and shall comply with and be located in accordance with **Table 10-I.4.2.1 (District & Civic Space Standards)**.

TABLE 10-I.4.8.2 VEHICULAR PARKING REQUIREMENTS

Principal Use Category	Number of Required Parking Spaces				
	CD-1	CD-3	CD-4	SD-AE	CS
Residential Dwelling Unit	NA	1.5 per DU	1 per DU	Per Planning Board	NA
Commercial - Lodging	NA	1 per Lodging Unit	1 per Lodging Unit	Per Planning Board	NA
Office	NA	3/1,000 sq ft	2/1,000 sq ft	Per Planning Board	NA
Commercial - other than Lodging	NA	Per Planning Board	Per Planning Board	Per Planning Board	NA
All other Use categories	NA	Per Planning Board			NA

- 2. Vehicular Parking required to be available for each Lot or Building Site shall be determined based on the quantity of Principal Use(s) of the Lot or Building Site and the number of spaces available to the Lot or Building Site, as determined by this Section 10-

I.4.8.2 and **Table 10-I.4.8.2 (Vehicular Parking Requirements)**, with any fractional spaces being rounded down to the nearest whole number.

- 3. The number of spaces of Parking available to a Lot or Building Site is the sum of:
  - a. All spaces within the Lot or Building Site,
  - b. All spaces Adjacent to the Frontage Line on the same side of the right-of-way as the Lot or Building Site, and
  - c. If elected by the Applicant, all spaces within the same or an Adjacent Block within a public Parking Lot or Parking Structure or by a recorded Parking agreement, or Easement.
- 4. The number of spaces of Parking available to a Lot or Building Site must not be less than, nor more than 10% greater than, the number of spaces of Parking determined by **Table 10-I.4.8.2 (Vehicular Parking Requirements)** based on the quantity of Principal Use(s) on the Lot or Building Site, provided that the minimum number of spaces may be reduced pursuant to Section 10-I.4.8.3.
- 5. In determining compliance with this Section 10-I.4.8.2, the following shall not be counted:
  - a. Accessory Dwelling Units; and
  - b. Liner Buildings fewer than thirty (30) feet deep and no more than two (2) Stories.

10-I.4.8.3 Parking Reductions.

For purposes of this Section 10-I.4.8, the number of Parking spaces required to be available to a Lot or Building Site may be reduced, at the election of the Applicant, as follows:

- 1. By dividing the number of spaces of Parking required to be available to the Lot or Building Site by the applicable Shared Parking Factor. The applicable Shared Parking Factor is determined under **Table 10-I.4.8.3 (Shared Parking Factor)** for any two (2) Principal Uses within the Lot or Building Site or within

## SECTION 10-1.4

the Lot or Building Site and any other Lot or Building Site within the same or any Adjacent Block.

### TABLE 10-I.4.8.3 SHARED PARKING FACTOR

The diagram is a diamond shape composed of a grid of cells. The central cell is labeled '1'. The four cells immediately surrounding the center are labeled '1.1'. The next layer of cells consists of four cells labeled '1.2' (top and bottom) and two cells labeled '1.3' (left and right). The outermost layer consists of four cells labeled '1.4' (top and bottom) and two cells labeled '1.7' (left and right). The four land uses are labeled at the corners: 'Residential' at the top, 'Commercial - Lodging' at the bottom, 'Office' on the left, and 'Commercial - other than Lodging' on the right.

2. The total number of Parking spaces required to be available to a Lot or Building Site may be further reduced by the following amounts in accordance with the following, which may be applied in any combination:

- a. Proximity to Public Transit or Commercial Shuttle Stop.

- (1) By seventy-five percent (75%) if any portion of the Lot or Building Site is within 1/2 mile of a Public Transit or Commercial Shuttle Stop;
- (2) By fifty percent (50%) if any portion of the Lot or Building Site is within 1 mile of a Public Transit or Commercial Shuttle Stop; and
- (3) By twenty-five percent (25%) if any portion of the Lot or Building Site is within 1 1/4 mile of a Public Transit or Commercial Shuttle Stop.

- b. Bicycle Parking.

- (1) By one (1) Parking space for every five (5) bicycle Parking spaces provided on the Lot or Building Site;
- (2) By five (5) Parking spaces for every indoor bicycle Parking storage area for ten (10) or more bicycles.

- c. Car-Sharing Parking Spaces.

- (1) By three (3) Parking spaces for each car-sharing Parking space available to the Lot or Building Site, up to a maximum reduction of twelve (12).

#### 10-I.4.8.4 Parking Location.

All Parking Areas, Parking Lots, Parking Structures, and Garages shall be located as specified for the applicable District in **Table 10-1.4.2.1 (District & Civic Space Standards)**.

### 10-I.4.8.5 Vehicular Access.

- 1.** Vehicular access to Parking Areas and Parking Lots shall be direct and not in conflict with general vehicular movement serving a site.
- 2.** Parking locations shall be accessed by Alleys when such are available on the applicable Plan or are otherwise available.

### 10-I.4.8.6 Vehicular Entrances & Exits.

Vehicular entrances and exits to Parking Areas, Parking Lots, and Parking Structures shall be designed to be easily identifiable by drivers and pedestrians. Any signage shall be simple, clear, and concise. Any gates, arms, or booths shall be set back at least twenty (20) feet from the Facades of Buildings.

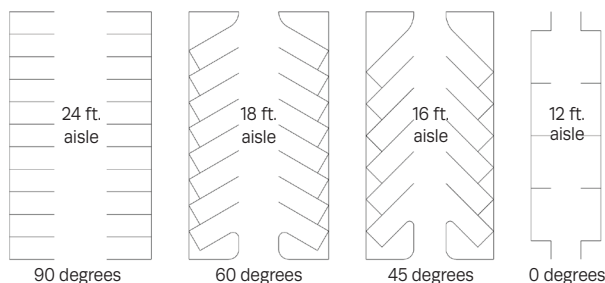
### 10-I.4.8.7 Design.

1. In all Districts, Parking Lots shall be designed in accordance with this Section 10-I.4.8.7 and other applicable provisions of this Article 10-I.4.
2. Except as otherwise provided for compact vehicle Parking accommodations under Section 10-I.4.8.7.3, the dimensional standards set forth in **Table 10-I.4.8.7.2 (Off-Street Vehicular Parking Space Dimensions)** are applicable to off-street Parking accommodations.

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**TABLE 10-I.4.8.7.2 OFF-STREET VEHICULAR PARKING SPACE DIMENSIONS**

Parking Angle (degrees)	Stall Width	Minimum Stall Length	Aisle Width
0	8 ft.	21 ft.	12 ft.
30 – 53	8 ft. – 9 ft.	18 ft. except 19 ft. if adjacent to curb	16 ft.
54 – 74	8 ft. – 9 ft.	18 ft. except 19 ft. if adjacent to curb	18 ft.
75 – 90	8 ft. – 9 ft.	18 ft. except 19 ft. if adjacent to curb	24 ft.



3. Any compact car spaces must be separately signed and marked in distinct, separate areas. The dimensional requirements for compact car spaces area as set forth in **Table 10-I.4.8.7.3 (Off-Street Compact Vehicle Parking Space Dimensions)**:

**TABLE 10-I.4.8.7.3 OFF-STREET COMPACT VEHICLE PARKING SPACE DIMENSIONS**

Parking Angle (degrees)	Space Width	Length of Space	Aisle Width
0	7.5 ft.	---	---
45	7.5 ft.	17 ft.	12 ft.
60	7.5 ft.	18 ft.	15 ft.
90	7.5 ft.	16 ft.	21 ft.

4. Any ramps leading from a Thoroughfare or Internal Drive to a Parking Lot, Parking Area, Garage, or Parking Structure shall be at least twenty (20) feet wide for two-way traffic and ten (10) feet wide for one-way traffic.

5. Curbs must be made of granite or concrete at heights between six (6) inches and eight (8) inches

above the elevation of the Thoroughfare or Internal Drive and must be at least six (6) inches wide.

6. Curb cuts must be at least twenty (20) feet wide for two-way access and ten (10) feet wide for one-way access. Except as otherwise expressly provided in this Zoning Ordinance with respect to numbers of Driveways, Lots and Building Sites shall be limited to one curb cut per Lot or Building Site.

7. Unless alternative Parking surfaces are otherwise permitted by this Zoning Ordinance, all off-street Parking Areas and Parking Lots shall be surfaced with asphalt or concrete. Asphalt Parking Areas and Parking Lots shall be compacted after sub-grade with a minimum of six (6) inch stone base, a minimum two (2) inches of modified asphalt binder, and a minimum one and one-half (1.5) inches of asphalt mix topping and must follow the construction detail provided below. Concrete Parking Areas and Parking Lots shall be constructed according to recognized construction standards. Construction plans for all Parking Areas and Parking Lots shall be subject to review and approval by the Town Engineer.

8. All Parking accommodations counted toward the applicable Parking requirements shall be paved and completed prior to issuance of the Final Certificate of Occupancy for the related Building(s).

9. Any Parking Area or Parking Lot in the First or Second Layer shall be Screened from view in accordance with 10-I.4.8.8.

10. Any Parking Area or Parking Lot having five (5) or more spaces shall include curbs, paving, Sidewalks, drainage facilities and lighting in accordance with this Zoning Ordinance and all other applicable laws, regulations, and specifications.

11. The maximum grade permitted for any required Parking shall not exceed 8%.

12. Except in District CD-3 and single-family Houses in CD-4, all off-street Parking spaces shall be striped with paint or some other form of permanent marking.



## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**13.** All access to any Town street from a Driveway shall have a paved Driveway apron as approved by the Town Engineer.

**14.** Any Parking Area or Parking Lot having ten (10) or more Parking spaces shall conform to the following:

a. Parking Areas and Parking Lots shall provide adequate lighting as determined by the Planning Board.

b. Parking Areas and Parking Lots shall contain at least one (1) landscape island for every ten (10) Parking spaces. Parking Lots with more than one (1) landscape island shall have such islands distributed throughout the Parking Lot.

c. Using the standard of each large tree shading a circular area having a radius of fifteen (15) feet with the trunk of the tree as the center, there must be sufficient trees so that 35% of the Parking Area will be shaded. Small trees used for perimeter landscaping requirements may be counted towards shading as described in **Appendix A**. Parking Structures may substitute any shading materials in lieu of trees but shall meet the shading percentage threshold described above.

d. Interior Parking rows shall be terminated at both ends with landscape islands.

e. Each Parking island shall be a minimum size equal to a standard Parking space; provided that each Parking island abutting two (2) rows of head to head Parking spaces shall be a minimum size and length equal to two (2) Parking spaces.

f. Parking islands shall contain a minimum of one (1) shade tree for every single island. If a Parking island is double length or width, then two (2) shade trees shall be required.

g. Rows of Parking fronting on drive aisles including Alleys shall be provided with a minimum five (5) feet (excluding curbs) landscape buffer.

h. Root zones for existing trees to remain shall be a minimum of forty-eight (48) square feet.

i. For every two-thousand (2,000) square feet of Parking Area or Parking Lot, at least one (1) tree shall be installed or preserved within the Parking Area or Parking Lot except to the extent that trees outside of the Lot or Building Site containing the Parking Area or Parking Lot are allowed to satisfy this requirement as set forth below.

j. No Parking space shall be more than seventy-two (72) feet from a tree within the Lot or Building Site, as measured from the center of the tree to the nearest line demarcating the space.

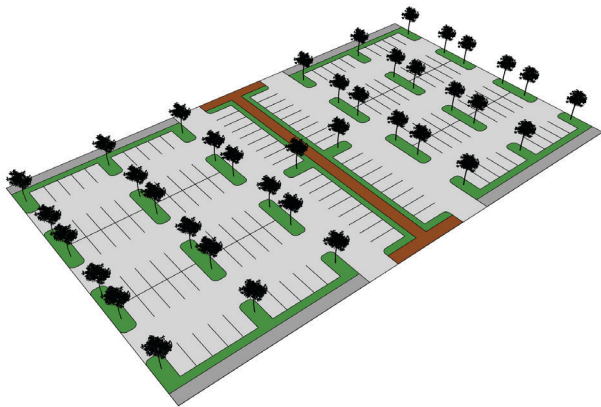
k. Except for trees allowed to be counted outside the Parking Area or Parking Lot, new trees shall be installed and/or existing trees preserved in tree islands provided pursuant to this Section and/or at the perimeter of the Parking Area or Parking Lot, provided that the perimeter of the Parking Area or Parking Lot where trees are installed or preserved to meet this requirement lies within the Lot or Building Site on which the Parking Area or Parking Lot is located.

l. Trees outside of the Parking Area or Parking Lot located within twenty (20) feet of the closest portion of such Parking Area or Parking Lot, including but not limited to trees within Thoroughfare or Internal Drive rights-of-way and Civic Spaces, may be counted toward satisfying the requirements.

m. In addition to any walkway or Sidewalk around a Parking Area or Parking Lot, each such Parking Area or Parking Lot exceeding one hundred and twenty (120) spaces shall have at least one (1) pedestrian walkway bisecting the Parking Area or Parking Lot and connecting to adjoining Sidewalks of a minimum width of eight (8) feet that is paved differently from the Parking spaces with respect to texture, material, style, and/or color.

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

### ILLUSTRATION 10-I.4.8.7 PARKING AREA / PARKING LOT LANDSCAPING



**15.** Parking Areas and Parking Lots shall include curbs, paving, Sidewalks, drainage facilities, and lighting in accordance with this Section as well as any other applicable laws, regulations, and specifications.

**16.** Parking Area paving shall be confined to the minimum area necessary to comply with the parking requirements of this Section.

**17.** Any Plan submitted under this Article shall show the proposed arrangement of Parking accommodations, including access to such areas from the Thoroughfares and Internal Drives and Internal Drives, with Parking spaces to comply with the requirements of this Section 10-I.4.8.7.

**18.** Parking Structures shall comply with the following:

- a. Parking Structures must be Screened from view of any Frontage by one (1) or more Liner Buildings as set forth in Section 10-I.4.8.8 and **Table 10-I.4.2.1 (District & Civic Space Standards)**. Such Liner Buildings must comply with Section 10-I.4.8.11.
- b. Parking Structures shall not exceed the maximum Building Height applicable to Buildings within the applicable District.
- c. Parking Structures shall be set back from the Thoroughfare or Internal Drive a minimum of twenty-four (24) feet.

d. Lots and Building Sites on which Parking Structures are to be located and the standards for such Lots and Building Sites are subject to approval by the Planning Board.

**19.** Lighting illuminating off-Street Parking accommodations in all Districts must be directed only within the applicable Parking Area, Parking Lot, Garage, or Parking Structure.

**20.** Handicapped-accessible Parking shall comply with the following:

- a. Handicapped-accessible Parking shall be located as near as possible to the main public entrances of a single Building or centrally located in Parking Areas that serve more than one (1) Building.
- b. All off-Street handicapped-accessible Parking spaces shall be located in the closest Parking Area to a public entrance to the Building but no more than two-hundred and fifty (250) feet from such entrance.
- c. No stairs or curbs are permitted between a handicapped-accessible Parking space and the entrance which it is intended to serve, and the slope along the accessible route shall not exceed 1:12.
- d. All off-Street handicapped-accessible Parking spaces shall be designated by a Sign or other means accepted by State requirements. Van-accessible spaces shall be designated by a Sign indicating "Van Accessible" in addition to any other means used.
- e. A minimum width of ninety-six (96) inches shall be required for all van-accessible spaces. Van-accessible spaces shall further require a minimum vertical clearance of ninety-eight (98) inches.
- f. An access aisle of sixty (60) inches shall be provided for all handicapped-accessible spaces and an aisle of ninety-six (96) inches shall be provided for all van-accessible spaces. One ninety-six (96) inch aisle may serve both types of spaces. No ramps may project into this access aisle.

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

21. No required Parking accommodation shall be Encroached upon by any Building, Structure, or Use.

22. All Parking Areas shall be screened from abutting Residential Uses.

### 10-I.4.8.8 Screens and Screening.

Screens and Streetscreens shall comply with the standards and requirements of **Table 10-I.4.2.1 (District & Civic Space Standards)**.

### 10-I.4.8.9 Driveway Width.

Driveways at Frontages shall be no wider in the First Layer than the width specified for the applicable District in **Table 10-I.4.2.1 (District & Civic Space Standards)**.

### 10-I.4.8.10 Pedestrian Exits.

Pedestrian exits from all Parking Lots, Parking Areas, Garages, and Parking Structures shall conform to **Table 10-I.4.2.1 (District & Civic Space Standards)**, except for underground Parking accommodations.

### 10-I.4.8.11 Liner Buildings Required.

All sides of a Parking Structure that face a Public Frontage shall be Enfronted by one (1) or more Liner Buildings complying with all standards applicable in the District as set forth in **Table 10-I.4.2.1 (District & Civic Space Standards)** and the following:

Liner Buildings shall:

1. be at least as tall as the associated Parking Structure which they Screen; and
2. have Shopfront Frontages at all Private Frontages.

## SECTION 10-I.4.9

### LOADING, SERVICE, STORAGE, TRASH RECEPTACLE, EQUIPMENT, & OTHER ITEMS LOCATIONS & REQUIREMENTS.

#### 10-I.4.9.1 General.

Trash receptacle and truck and bus, as applicable, loading space, shall be located and provided as specified for the applicable District in **Table 10-I.4.2.1 (District & Civic Space Standards)**.

#### 10-I.4.9.2 Locations of Certain Items.

All outdoor loading, storage, service, heating, ventilation and air-conditioning equipment, utility service meters and equipment, mechanical equipment, antennas and satellite equipment, solar energy equipment, and clothes drying equipment, animal enclosures, runs, shelters, and equipment, and swimming pool, hot tub and spa locations shall be as specified for the applicable District, as required in **Table 10-I.4.2.1 (District & Civic Space Standards)**.

#### 10-I.4.9.3 Vehicular Access.

Vehicular access to trash receptacle and loading space shall be direct and not in conflict with general vehicular movement serving a site.

Loading, service, storage, and trash receptacle locations shall be accessed by Alleys, when such are available on the applicable Plan or otherwise available.

#### 10-I.4.9.4 Vehicular Entrances & Exits.

Vehicular entrances and exits shall be designed to be easily identifiable by drivers and pedestrians. Any signage shall be simple, clear, and concise. Any gates, arms, or booths shall be set back at least twenty (20) feet from the Facades of Adjacent Buildings.

#### 10-I.4.9.5 Screening Required.

All loading areas shall be Screened from Residential Uses.

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

### SECTION 10-I.4.10


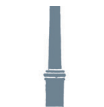
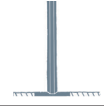





#### PRIVATE & PUBLIC LIGHTING.

##### 10-I.4.10.1 Private & Public Lighting Fixtures.




Private & Public Lighting fixtures and their poles shall comply with the following, except for those that are attached to Buildings:

1. **Table 10-I.4.10.1-A (Private Lighting Types)** and **Table 10-I.4.10.1-B (Public Lighting Types)**, except those that are attached to Buildings.
2. Lighting standards shall be no higher than twenty (20) feet in Non-Residential Parking Areas and Parking Lots or fifteen (15) feet in other areas.
3. Lighting fixtures shall be mounted in a manner so that the cone of illumination is contained on-site and does not cross any property line of the site.
4. All lighting fixtures shall be full cut-off.
5. Lighting shall not be oriented so that it directs glare or excessive illumination onto Thoroughfares and Internal Drives in a manner that may distract or interfere with the vision of drivers.
6. Any fixtures used to accent architectural features, landscaping or art shall be located, aimed, or shielded to minimize light spill into the night sky.
7. Use of illuminated tubing or light strings outlining or defining property lines, sales areas, roof lines, doors, windows or similar areas or features in a manner that is not primarily for safety purposes, as determined by the Code Enforcement Officer, is prohibited. This paragraph shall not limit the use of string lights illuminating outdoor dining, seasonal lighting or gathering areas.
8. Other variations of private and public lighting types are allowed with Planning Board approval.

**TABLE 10-I.4.10.1-A PRIVATE LIGHTING TYPES**









LIGHTING TYPE	CD-1	CD-3	CD-4	SD-AE	CS
<b>POLE TYPES</b>					
Fiberglass Pole 	NR	NP	NP	NP	NA
Aluminum Pole 	NR	P	P	P	NA
Octagonal Concrete Pole 	NR	P	P	P	NA
Fluted Concrete Pole 	NR	P	P	P	NA
<b>HEAD TYPES</b>					
Cobra Head 	NR	NP	NP	NP	NA
Colonial Head 	NR	P	P	P	NA
Coach Head 	NR	P	P	P	NA
Acorn Head 	NP	NP	NP	NP	NP

**LEGEND** The following notations are utilized in this table.




-  Permitted as of Right
  Not Regulated  
 Not Permitted

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**TABLE 10-I.4.10.1-B PUBLIC LIGHTING TYPES**

LIGHTING TYPE	CD-1	CD-3	CD-4	SD-AE	CS
<b>POLE TYPES</b>					
Fiberglass Pole 	NR	NR	NP	NP	NP
Aluminum Pole 	NR	NR	P	P	P
Octagonal Concrete Pole 	NR	NR	P	P	P
Fluted Concrete Pole 	NR	NR	P	P	P
<b>HEAD TYPES</b>					
Cobra Head 	NR	NR	NP	NP	NP
Colonial Head 	NR	NR	P	P	P
Coach Head 	NR	NR	P	P	P
Acorn Head 	NP	NP	NP	NP	NP

**LEGEND** The following notations are utilized in this table.

-  Permitted as of Right
  Not Regulated
  Not Permitted

### 10-I.4.10.2 Private & Public Lighting Standards.

The lighting level measured at the Frontage Lines must comply with the applicable intensity indicated for the applicable District in **Table 10-I.4.10.2 (Private & Public Lighting Standards)**.

**TABLE 10-I.4.10.2 PRIVATE & PUBLIC LIGHTING STANDARDS**

District	Min/Max Lighting Level at Frontage Line (in foot-candles)
CD-1	Not Regulated
CD-3	0-0.2 fc
CD-4	0-1.0 fc
SD-AE	0-1.0 fc
CS	Not Regulated

### SECTION 10-I.4.11 THOROUGHFARES, INTERNAL DRIVES, ALLEYS, PEDESTRIAN WAYS & BIKEWAYS.

#### 10-I.4.11.1 Thoroughfares & Internal Drives – General.

1. Thoroughfares and Internal Drives shall be provided within and Adjacent to each Development Site as necessary to comply with this Section 10-I.4.11.
2. For any Development Site of five (5) or more acres of Net Developable Site Area, a Thoroughfare or Internal Drive network shall be laid out.
3. Thoroughfares and Internal Drives shall:
  - a. be maintained by the property owner;
  - b. provide for vehicular and non-vehicular traffic and provide access to Lots, Building Sites, and Open Spaces;
  - c. terminate at other Thoroughfares or Internal Drives, forming a network, with Thoroughfares and Internal Drives connecting wherever possible to those on Abutting sites;

## SECTION 10–I.4: STANDARDS & REQUIREMENTS

- d. not include cul-de-sacs, which are prohibited;
- e. be designed to define Blocks not exceeding the applicable perimeter size prescribed in **Table 10–I.4.11.1 (Block Perimeter Standards)**, measured as the sum of Lot or Building Site Frontage Lines and subject to adjustment as necessary at the edge of a Development Site; and
- f. comply with all applicable ADA standards.

**TABLE 10–I.4.11.1 BLOCK PERIMETER STANDARDS**

District	Max Block Perimeter
CD-1	Not Regulated
CD-3	3,000 ft. max.; 800 ft. max. for any side. Pedestrian Path with Bikeway may be used to break up block.
CD-4	2,000 ft. max., or 3,000 ft. max. if Parking Structure is provided within the Block; 800 ft. max. for any side
SD-AE	Per CD-4 standard, except <b>NR</b> for any A & E Area
CS	Per applicable District standard

- 4. Alleys, where provided, shall provide for vehicular access to the side or rear of Lots, Building Sites, and Civic Spaces.

### 10–I.4.11.2 Thoroughfare & Internal Drive Types & Standards.

- 1. New Thoroughfares and Internal Drives, and extensions or changes to any existing Thoroughfares and Internal Drives to the best practical extent possible as determined by the Planning Board, shall:
  - a. include the components and comply with the standards and requirements for the applicable Thoroughfare or Internal Drive type (see **Illustration 10–I.4.11.2.1A (Thoroughfare or Internal Drive Components)**, as provided in **Table 10–I.4.11.2.1B (Thoroughfare or Internal Drive Types – Summary)** and **Table 10–I.4.11.2.1C (Thoroughfare or Internal Drive Assemblies & Standards)**; and

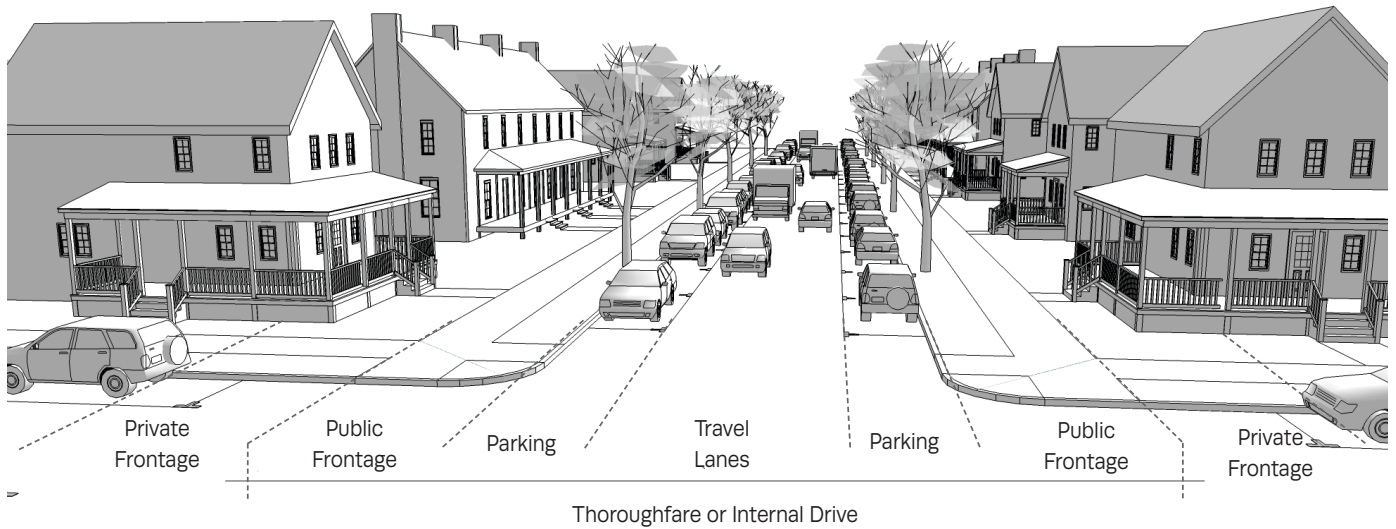
- b. except for Alleys, have Public Frontages that adjust accordingly as they pass from one District to another or, alternatively, follow the alignment of the Thoroughfare or Internal Drive to the depth of one Lot or Building Site retaining a single Public Frontage throughout the entire trajectory.

- 2. Where a Driveway connects to a collector or major Thoroughfare or Internal Drive, it shall be designed to provide for a turnaround to avoid backing into the Thoroughfare or Internal Drive.
- 3. All Major Subdivisions or any commercial development within a CD-4 or SD-AE District requires improvements to existing and directly abutting Thoroughfares and Internal Drives.
- 4. This section is not intended to apply to general maintenance.



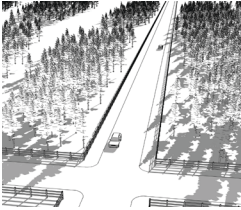
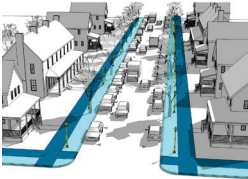
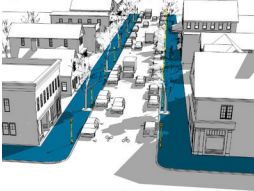
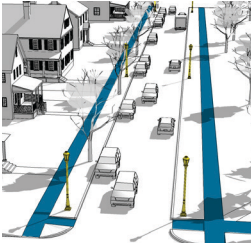
## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**ILLUSTRATION 10-I.4.11.2.1A THOROUGHFARE OR INTERNAL DRIVE COMPONENTS**



# SECTION 10-I.4: STANDARDS & REQUIREMENTS

TABLE 10-I.4.11.2.1B THOROUGHFARE & INTERNAL DRIVE TYPES – SUMMARY

Thoroughfare Type	Illustration	Permitted Districts
<p><b>Road</b></p> <p>A local, rural and suburban Thoroughfare of low-to-moderate vehicular speed and capacity, and having a Public Frontage that is drained by swales. There are continuous Planters and no on-street parking. The landscaping consists of clustered and irregularly spaced trees along the Thoroughfare.</p>		<div>CD-1</div>
<p><b>Neighborhood Street</b></p> <p>A local urban Thoroughfare of low speed and capacity and having a Public Frontage with raised Curbs drained by inlets, Sidewalks separated from the Vehicular Lanes by individual or continuous Planters, and parking on one or both sides. The landscaping consists of regularly spaced and aligned rows of trees along the Thoroughfare.</p>		<div>CD-3</div> <div>CD-4</div> <div>SD-AE</div>
<p><b>Commercial Street</b></p> <p>A Thoroughfare type designed for moderate to high vehicular capacity and slow speed, traversing an urbanized area. The Public Frontage has raised Curbs drained by inlets and very wide Sidewalks along both sides, separated from the Vehicular Lanes by separate tree wells or planters with grates and parking on both sides. The landscaping consists of regularly spaced and aligned rows of trees along the Thoroughfare, which clears the storefront entrances.</p>		<div>CD-4</div> <div>SD-AE</div>
<p><b>Drive</b></p> <p>A Thoroughfare type with Building Frontages on one side and no or very sporadic Building Frontages on the other side. Frequently, a Drive is used along a boundary between an urbanized area and a natural condition along a waterfront or Natural Area. The Public Frontage has raised Curbs drained by inlets and a wide Sidewalk or paved Path along one side, related to a greenway or waterfront. It is separated from the Vehicular Lanes by individual or continuous planters. The landscaping consists of Thoroughfare trees aligned in a regularly spaced row.</p>		<div>CD-3</div> <div>CD-4</div>

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**TABLE 10-I.4.11.2.1C THOROUGHFARE & INTERNAL DRIVE ASSEMBLIES & STANDARDS**  
**ROAD**



Illustration is provided for illustrative purposes only.

### Permitted Districts

#### CD-1

Thoroughfare or Internal Drive Type	Road	
Right of Way Width	29-31 ft	A
Pavement Width	20-22 ft	B
Movement	Slow	
<b>Assemblies</b>		
Travel Lanes	2 Lanes	C
Travel Lane Width	10-11 ft	
Parking Lanes	None	
Parking Lane Width	N/A	
Bikeway Type	Shared Use Lane. See Table 10-I.4.11.5*	D

\* Bikeways are mandatory. Right of way and pavement width shall be increased by the aggregate width of bikeways, in accordance with Table 10-I.4.11.5 (Bikeway Types).

<b>Pedestrian Way</b>		
Pedestrian Way Type	Multi-Use Trail, Path. Walkway not required to be contiguous to Road.	
Pedestrian Way Width	Per Walkway Type	

<b>Planter</b>		
Planter Type	Continuous Swale	E
Planter Width	9 ft	
Landscape Type	Not Regulated	
<b>Curb</b>		
Turning Radius	25 ft	
Curb Radius	25 ft	F
Curb Type	Rural Edge Treatment or Ribbon Curb	
<b>Lighting</b>		
Placement & Type	Type and design approved by Public Works and in accordance with Decision-Making Authority	

Thoroughfare specifications shall meet engineering standards and be subject to approval by the Town Engineer.

# SECTION 10-I.4: STANDARDS & REQUIREMENTS

TABLE 10-I.4.11.2.1C THOROUGHFARE & INTERNAL DRIVE ASSEMBLIES & STANDARDS  
NEIGHBORHOOD STREET

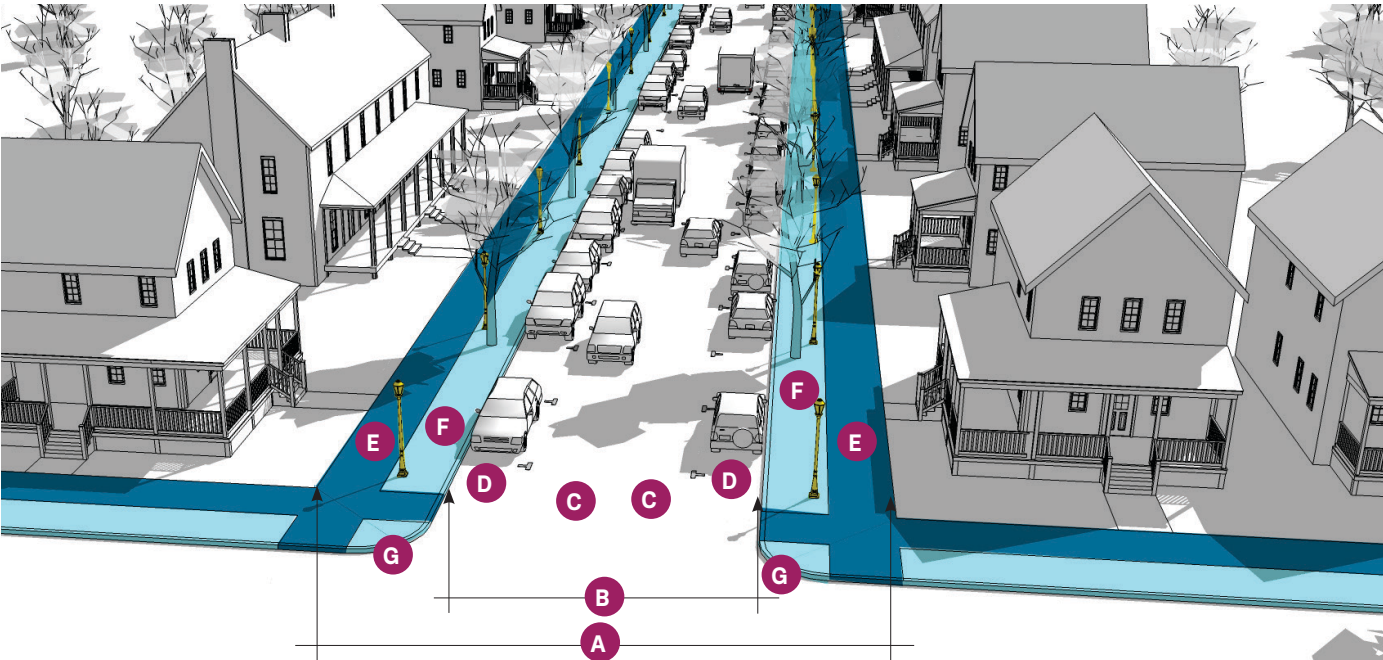


Illustration is provided for illustrative purposes only.

## Permitted Districts

**CD-3** **CD-4** **SD-AE**

Thoroughfare or Internal Drive Type	Neighborhood Street	
Right of Way Width	50 – 76 ft	A
Pavement Width	28 ft – 36 ft	B
Movement	Slow	
Assemblies		
Travel Lanes	2 Lanes	C
Travel Lane Width	10 ft	
Parking Lanes	Parallel, 1 or both sides	D
Parking Lane Width	8 ft.	
Bikeway Type	Shared Use Lane. See Table 10-I.4.11.5*	
* Bikeways are mandatory. Right of way and pavement width shall be increased by the aggregate width of bikeways, in accordance with Table 10-I.4.11.5 (Bikeway Types).		
Pedestrian Way		
Pedestrian Way Type	Sidewalk. Both sides.	E
Pedestrian Way Width	6 – 8 ft	

Planter		
Planter Type	Continuous Planter	F
Planter Width	5 – 12 ft	
Landscape Type	Trees at 30 ft on center average; first tree shall be placed within 30 ft of Block corner.	
Curb		
Turning Radius	25 ft min	
Curb Radius	10 –20 ft*, except where travel lane is adjacent to the curb, the maximum curb radius shall be 25 ft.	G
Curb Type	Raised Curb (Travel Lane may not include gutter, Parking Lane may)	
* Designated truck routes may have a curb radius larger than 20 feet if required by the Department of Public Works to accommodate the design vehicle for that route.		
Lighting		
Placement & Type	Type and design approved by Public Works and in accordance with Decision-Making Authority	

Thoroughfare specifications shall meet engineering standards and be subject to approval by the Town Engineer.



## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**TABLE 10-I.4.11.2.1C THOROUGHFARE & INTERNAL DRIVE ASSEMBLIES & STANDARDS  
COMMERCIAL STREET**

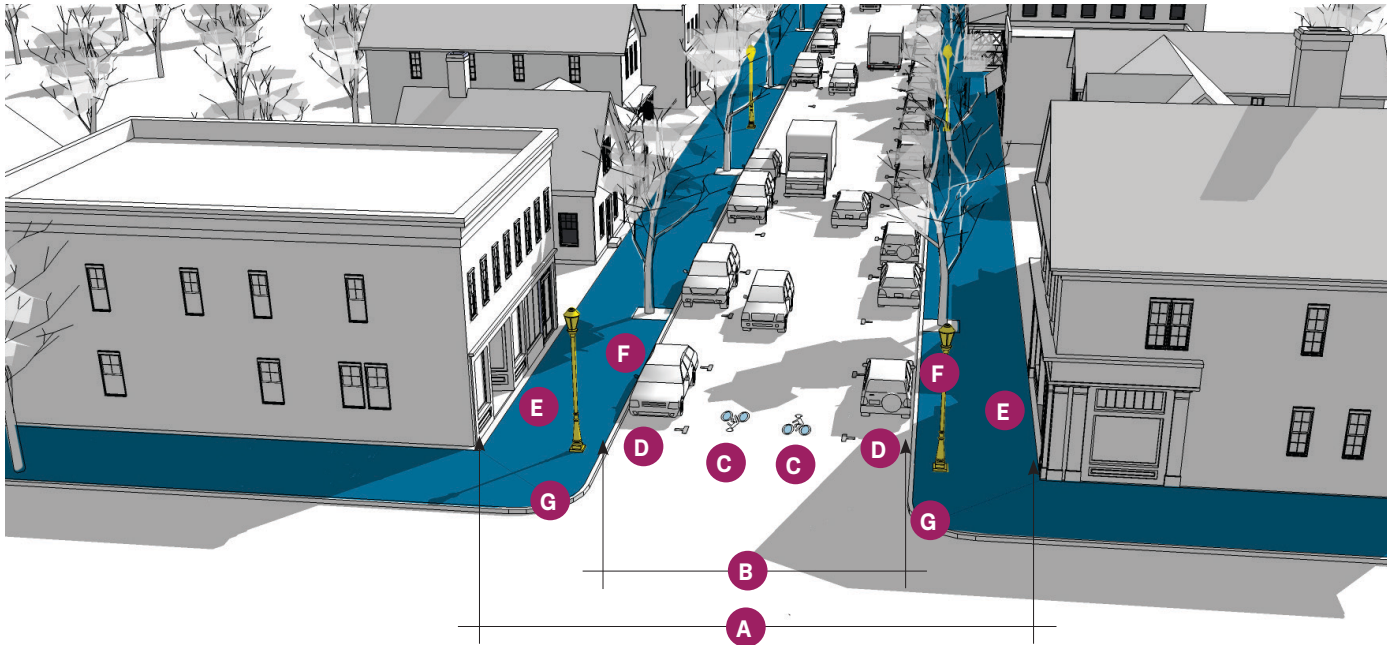


Illustration is provided for illustrative purposes only.

### Permitted Districts

**CD-4 | SD-AE**

Thoroughfare or Internal Drive Type	Commercial Street	
Right of Way Width	56 – 88 ft	<b>A</b>
Pavement Width	60 – 76 ft	<b>B</b>
Movement	Slow	
<b>Assemblies</b>		
Travel Lanes	2 Lanes	<b>C</b>
Travel Lane Width	10 ft	
Parking Lanes	Parallel, 2 sides	<b>D</b>
Parking Lane Width	8 ft, marked	
Bikeway Type	Shared Use Lane, Protected Bicycle Lane. See <b>Table 10-I.4.11.5*</b>	
* Bikeways are mandatory. Right of way and pavement width shall be increased by the aggregate width of bikeways, in accordance with <b>Table 10-I.4.11.5 (Bikeway Types)</b> .		
<b>Pedestrian Way</b>		
Pedestrian Way Type	Sidewalk, Passage. Both sides.	<b>E</b>
Pedestrian Way Width	6 – 20 ft	

<b>Planter</b>		
Planter Type	Tree Well or Planter	<b>F</b>
Planter Width	4 – 6 ft	
Landscape Type	Trees at 30 ft on center average; first tree shall be placed within 30 ft of Block corner.	
<b>Curb</b>		
Turning Radius	25 ft min	
Curb Radius	5 – 20 ft*, except where travel lane is adjacent to the curb, the maximum curb radius shall be 25 ft.	<b>G</b>
Curb Type	Raised Curb (Travel Lane may not include gutter, Parking Lane may)	
*Designated truck routes may have a curb radius larger than 20 feet if required by the Department of Public Works to accommodate the design vehicle for that route.		
<b>Lighting</b>		
Placement & Type	Type and design approved by Public Works and in accordance with Decision-Making Authority	

Thoroughfare specifications shall meet engineering standards and be subject to approval by the Town Engineer.

SECTION 10-I.4: STANDARDS & REQUIREMENTS

TABLE 10-I.4.11.2.1C THOROUGHFARE & INTERNAL DRIVE ASSEMBLIES & STANDARDS  
DRIVE

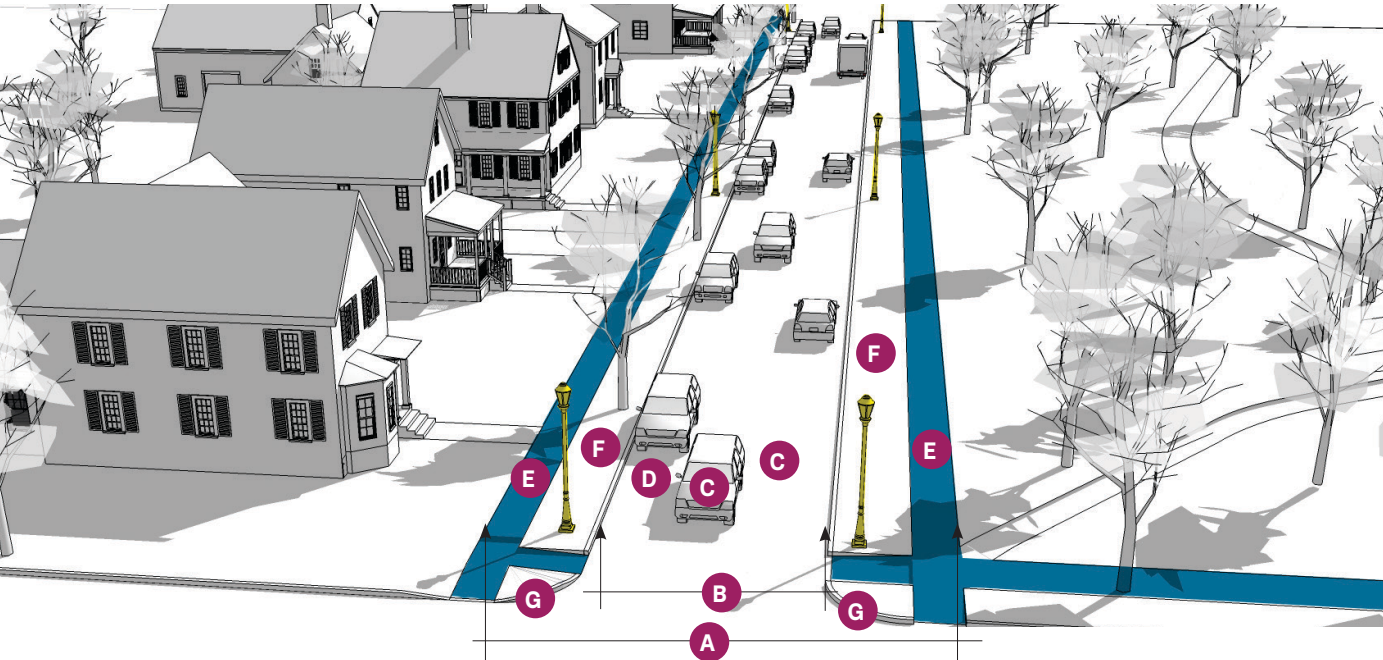


Illustration is provided for illustrative purposes only.

Permitted Districts

CD-3 CD-4

Thoroughfare or Internal Drive Type	Drive	
Right of Way Width	46 – 72 ft	A
Pavement Width	34 – 48 ft	B
Movement	Slow	
Assemblies		
Travel Lanes	1 – 2 lanes	
Travel Lane Width	10 ft	C
Parking Lanes*	Parallel, 1 side	D
Parking Lane Width*	8 ft., unmarked	
Bikeway Type	Shared Use Lane, Protected Bicycle Lane. See Table 10-I.4.11.5*	
* Bikeways are mandatory. Right of way and pavement width shall be increased by the aggregate width of bikeways, in accordance with Table 10-I.4.11.5 (Bikeway Types).		
Pedestrian Way		
Pedestrian Way Type	Sidewalk, Multi-Use Trail. Both sides.	E
Pedestrian Way Width	6 – 10 ft	

\*Parking Lanes not required on a Drive in CD-3.

Planter		
Planter Type	Continuous Planter	F
Planter Width	8 – 12 ft	
Landscape Type	Trees at 30 ft on center average; first tree shall be placed within 30 ft of Block corner.	
Curb		
Turning Radius	25 ft min	
Curb Radius	10 – 20 ft*, except where travel lane is adjacent to the curb, the maximum curb radius shall be 25 ft.	G
Curb Type	Raised Curb (Travel Lane may not include gutter, Parking Lane may)	
* Designated truck routes may have a curb radius larger than 20 feet if required by the Department of Public Works to accommodate the design vehicle for that route.		
Lighting		
Placement & Type	Type and design approved by Public Works and in accordance with Decision-Making Authority	

Thoroughfare specifications shall meet engineering standards and be subject to approval by the Town Engineer.



## SECTION 10-I.4: STANDARDS & REQUIREMENTS

### 10-I.4.11.3 Alleys.

Each Alley shall conform to the elements shown in **Illustration 10-I.4.11.3 (Alley Components)**, and the assembly and standards of **Table 10-I.4.11.3 (Alley Assembly & Standards)**.

**ILLUSTRATION 10-I.4.11.3 ALLEY COMPONENTS**



SECTION 10-I.4: STANDARDS & REQUIREMENTS

TABLE 10-I.4.11.3 ALLEY ASSEMBLY & STANDARDS



Illustration is provided for illustrative purposes only.

Permitted Districts

CD-3 CD-4 SD-AE

Thoroughfare or Internal Drive Type		Rear Alley
Right of Way Width	24 ft max	A
Pavement Width	24 ft max	B
Movement	Slow	
Assemblies		
Travel Lanes	N/A	
Travel Lane Width	N/A	
Parking Lanes	None	
Parking Lane Width	N/A	
Bikeway Type	N/A	
Pedestrian Way		
Pedestrian Way Type	None	
Pedestrian Way Width	N/A	

Planter	
Planter Type	None
Planter Width	N/A
Landscape Type	None
Tree Species	N/A
Curb	
Turning Radius	Not Regulated
Curb Radius	None
Curb Type	None
Lighting	
Not Regulated	

### 10-I.4.11.4 Pedestrian Ways.

1. In addition to elements of Thoroughfares, Internal Alleys, Parking Lots, and Parking Areas, any Pedestrian Ways shall comply with the requirements shown in **Table 10-I.4.11.4 (Pedestrian Way Types)**.
2. Pedestrian Ways shall be provided from each Building entry to surrounding Thoroughfares and Internal Drives, external Sidewalks, transit stops, and out-parcels.
3. Pedestrian Ways shall be provided for access between Parking Lots and Parking Areas and each Building public entrance.
4. Pedestrian ways shall comply with all applicable ADA standards.
5. At least one (1) handicapped accessible route in accordance with applicable State requirements shall connect Buildings, facilities, elements and Parking spaces that are on the same Lot, Building Site, or Development Site.
6. Crosswalks shall be designed and provided for pedestrian access to and from Buildings, Parking Lots, and Parking Areas, and shall be clearly designated with signage and variations in pavement materials or markings.

# SECTION 10-I.4: STANDARDS & REQUIREMENTS

TABLE 10-I.4.11.4 PEDESTRIAN WAY TYPES

## Multi-Use Trail

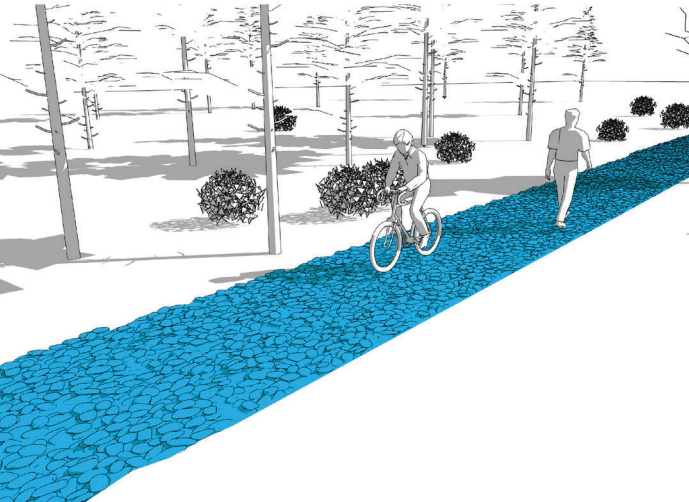


Illustration is provided for illustrative purposes only.

### Permitted Districts

**CD-1** **CD-3\*** **CD-4\*** **SD-AE**

A Pedestrian Way type that accommodates pedestrian and bicycle movement and traverses a Natural Area or rural area, with landscape matching the contiguous Open Space, ideally connecting directly with the urban Sidewalk network.

Width	9 ft min
Material	CD-1: Pervious; CD-3 and CD-4: Not Applicable; SD-AE: Impervious

\*Multi-Use Trails permitted in CD-3 and CD-4 if not used for primary access to Uses or Structures. Multi-Use trails shall not be used to break up Blocks.

## Path

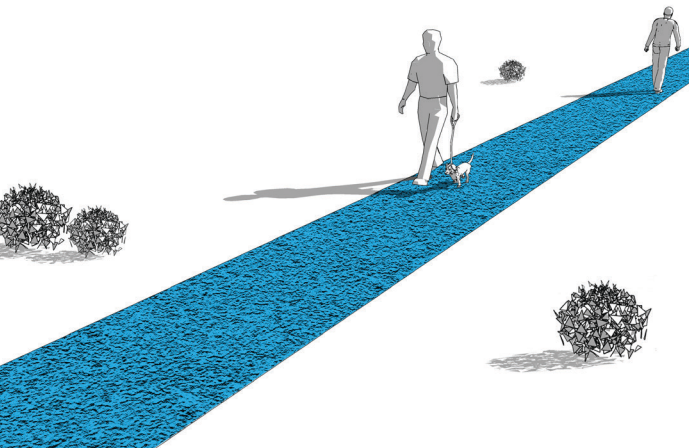


Illustration is provided for illustrative purposes only.

### Permitted Districts

**CD-1**

A Pedestrian Way traversing a Natural Area or rural area, with landscape matching the contiguous Open Space, ideally connecting directly with the urban Sidewalk network.

Width	6 ft min
Material	Pervious

TABLE 10-I.4.11.4 PEDESTRIAN WAY TYPES

Sidewalk

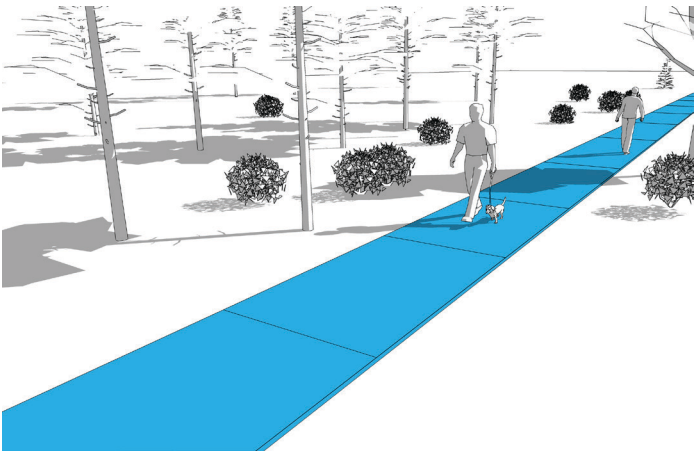


Illustration is provided for illustrative purposes only.

Permitted Districts

CD-3 CD-4 SD-AE

A Pedestrian Way type dedicated to pedestrian activity.

Width	6 ft min
Material	Impervious; Contiguous Sidewalks shall be in the same material and design.

Passage



Illustration is provided for illustrative purposes only.

Permitted Districts

CD-4 SD-AE

A Pedestrian Way, open or roofed, which passes between Buildings to provide shortcuts through long Blocks and connect rear Parking accommodations to Frontages.

Width	4 ft. min., 18 ft. max.; On-Passage Dining Permitted, provided 4 ft. min. left clear
Material	Pavers or Concrete

# SECTION 10-I.4: STANDARDS & REQUIREMENTS

## 10-I.4.11.5 Bikeways.

A bicycle network comprised of the various allowed Bikeway Types indicated in **Table 10-I.4.11.5 (Bikeway Types)** shall be provided throughout each Development Site of five (5) acres or more.

TABLE 10-I.4.11.5 BIKEWAY TYPES

### Bicycle Lane

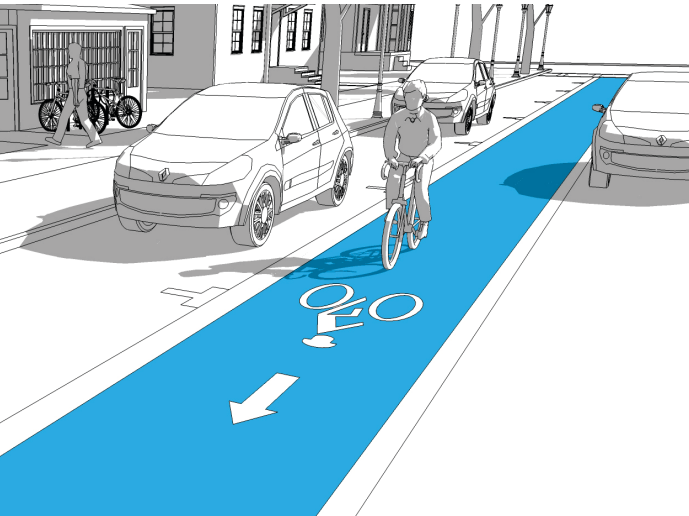


Illustration is provided for illustrative purposes only.

#### Permitted Districts

**CD-1** **CD-3** **CD-4** **SD-AE**

Riding Surface Width	5 ft min
Buffer	None
Movement	With traffic
Intersection Detailing	Signalized, Peg-a-Track, colored, Bicycle Box
Bicycle Parking	Rack, bicycle shelter, bicycle station

### Protected Bicycle Lane

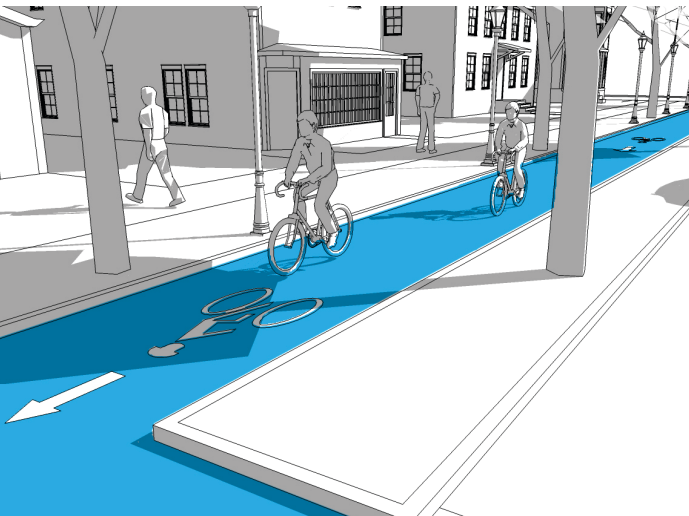


Illustration is provided for illustrative purposes only.

#### Permitted Districts

**CD-3** **CD-4** **SD-AE**

Riding Surface Width	5 ft min
Buffer	3 ft min, buffer, curb, or planter strip
Movement	With traffic or dual direction
Intersection Detailing	Signalized, Peg-a-Track, colored, Bicycle Box
Bicycle Parking	Rack, bicycle shelter, bicycle station



TABLE 10-I.4.11.5 BIKEWAY TYPES

Shared Use Lane

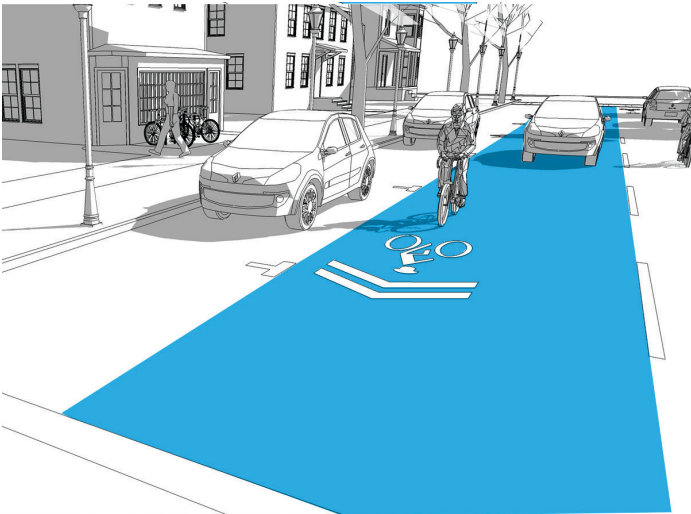


Illustration is provided for illustrative purposes only.

Permitted Districts

CD-3 CD-4 SD-AE

Riding Surface Width	Same as Vehicular Lane
Buffer	N/A
Movement	With traffic
Intersection Detailing	Signed, signalized
Bicycle Parking	Rack, Bicycle Shelter

Shared Use Path

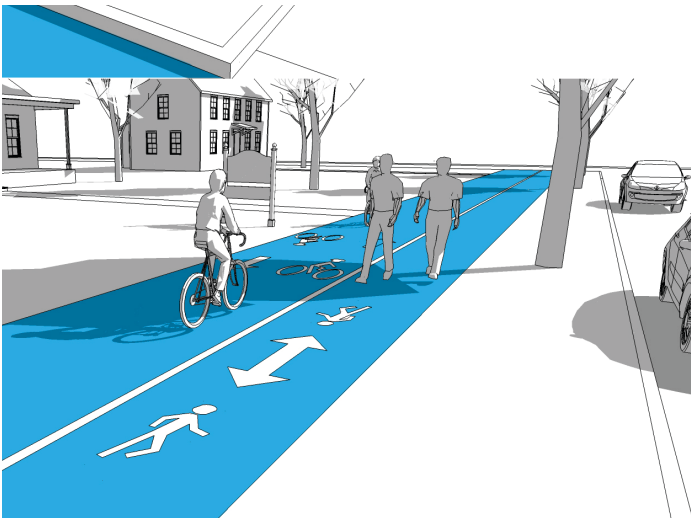


Illustration is provided for illustrative purposes only.

Permitted Districts

CD-1 CD-3 CD-4

Riding Surface Width	Limit to 1 bi-directional Bikeway per Right of Way; 10 ft. min.; or 1 uni-directional Bikeway on path on each side permitted
Buffer	3 ft min., if Buffer is between pavement surface and Bike Lane; if Buffer is the planting strip, then Character District minimums for tree planting width apply
Movement	With traffic or dual direction
Intersection Detailing	Signed, signalized, Peg-a-Track
Bicycle Parking	Rack, bicycle shelter, bicycle station

## SECTION 10–I.4: STANDARDS & REQUIREMENTS

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### SECTION 10–I.4.12

#### CIVIC SPACES & CIVIC BUILDINGS.

##### **10–I.4.12.1 Assignment.**

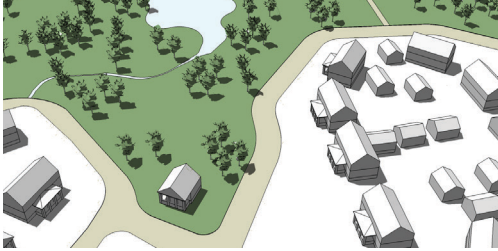


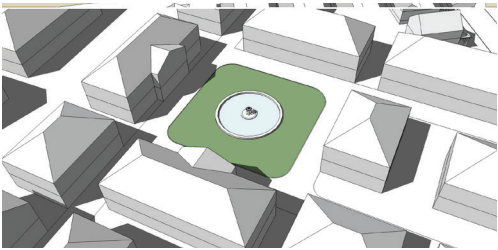
Civic Spaces shall be assigned according to Section 10–I.3.2.2.

##### **10–I.4.12.2 Civic Space Design.**

Civic Spaces shall be designed in accordance with Section **Table 10–I.4.12.2A (Civic Space Types – Summary)** and **Table 10–I.4.12.2B (Civic Space – Specific Standards)**.

# SECTION 10-I.4: STANDARDS & REQUIREMENTS




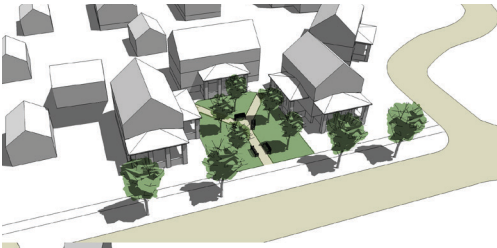
**TABLE 10-I.4.12.2A CIVIC SPACE TYPES – SUMMARY**

Civic Space Type	Illustration	Permitted In Districts
<p><b>Natural Area</b></p> <p>A natural preserve available for unstructured recreation. A Natural Area may be independent of surrounding Building Frontages. Its landscape shall consist of Paths and trails, meadows, waterbodies, woodland and open shelters, all naturalistically disposed. Natural Areas may be lineal, following the trajectories of natural corridors. The minimum size shall be 8 acres.</p>		CD-1 SD-AE*
<p><b>Green</b></p> <p>An Open Space, available for unstructured recreation. A Green may be spatially defined by landscaping rather than Building Frontages. Its landscape shall consist of lawn and trees, naturalistically disposed. The minimum size shall be 1/2 acre and the maximum shall be 8 acres.</p>		CD-3 CD-4 SD-AE*
<p><b>Square</b></p> <p>An Open Space available for unstructured recreation and Civic purposes. A Square is spatially defined by Building Frontages. Its landscape shall consist of paths, lawns and trees, formally disposed. Squares shall be located at the intersection of Thoroughfares or Internal Drives. The minimum size shall be 1/2 acre and the maximum shall be 5 acres.</p>		CD-3 CD-4 SD-AE*
<p><b>Plaza</b></p> <p>An Open Space available for Civic purposes and Commercial activities. A Plaza shall be spatially defined by Building Frontages. Its landscape shall consist primarily of pavement. Trees are optional. Plazas should be located at the intersection of important Thoroughfares or Internal Drives. The minimum size shall be 1/4 acre and the maximum shall be 2 acres.</p>		CD-4 SD-AE*

\*As indicated in Table 10-I.4.2.1

# SECTION 10-I.4: STANDARDS & REQUIREMENTS

TABLE 10-I.4.12.2A CIVIC SPACE TYPES – SUMMARY

Civic Space Type (continued)	Illustration	Permitted In Districts
<p><b>Playground</b></p> <p>An Open Space designed and equipped for the recreation of children. A Playground should be fenced and may include an open shelter. Playgrounds shall be interspersed within Residential areas and may be placed within a Block. Playgrounds may be included within Natural Areas and Greens. There shall be no minimum size and the maximum size shall be 2 acres.</p>		<div>CD-1</div> <div>CD-3</div> <div>CD-4</div> <div>SD-AE*</div>
<p><b>Sport Field or Court</b></p> <p>An open area designed and equipped for team sports activities.</p>		<div>CD-3</div> <div>CD-4</div> <div>SD-AE*</div>
<p><b>Community Garden</b></p> <p>A grouping of garden plots available for small-scale cultivation, generally to residents without private gardens. Community gardens should be fenced and accommodate individual storage sheds. Running water is required. Community Gardens shall be interspersed within Residential areas and may be placed within a Block or included within Natural Areas and Greens. There shall be no minimum or maximum size.</p>		<div>CD-3</div> <div>CD-4</div> <div>SD-AE*</div>
<p><b>Pocket Park</b></p> <p>A small predominantly green open area available for unstructured passive recreation.</p>		<div>CD-3</div> <div>CD-4</div> <div>SD-AE*</div>

*\*As indicated in Table 10-I.4.2.1*

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

TABLE 10-I.4.12.2B CIVIC SPACE – SPECIFIC STANDARDS

### Natural Area



#### Permitted In:

**CD-1 SD-AE\***

#### Intent

A natural preserve available for unstructured recreation. A Natural Area may be independent of surrounding Building Frontages. Its landscape shall consist of Paths and trails, meadows, waterbodies, woodland and open shelters, all naturalistically disposed. Natural Areas may be lineal, following the trajectories of natural corridors. The minimum size shall be 8 acres.

#### Specifications

Size	8 ac. min.
Frontage	10% of Natural Area must Enfront a Thoroughfare
Character	Natural

#### Typical Facilities

- Passive and active recreation
- Recreational and community facilities
- Playgrounds and play structures
- Paths and trails
- Accessory structures

### Green



#### Permitted In:

**CD-3 CD-4 SD-AE\***

#### Intent

An Open Space, available for unstructured recreation. A Green may be spatially defined by landscaping rather than Building Frontages. Its landscape shall consist of lawn and trees, naturalistically disposed. The minimum size shall be 1/2 acre and the maximum shall be 8 acres.

#### Specifications

Size	1/2 ac min., 8 ac. max.
Frontage	Independent; 10% of Green must Enfront a Thoroughfare
Character	Informal

#### Typical Facilities

- Passive and active recreation
- Playgrounds and play structures
- Paths and trails
- Accessory structures

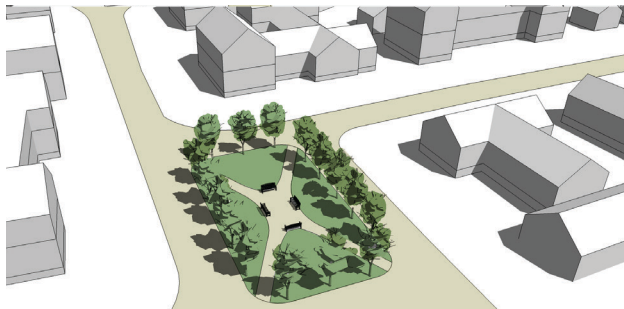
\*As indicated in Table 10-I.4.2.1



# SECTION 10-I.4: STANDARDS & REQUIREMENTS

TABLE 10-I.4.12.2B CIVIC SPACE – SPECIFIC STANDARDS

## Square



### Permitted In:

CD-3 CD-4 SD-AE\*

### Intent

An Open Space available for unstructured recreation and Civic purposes. A Square is spatially defined by Building Frontages. Its landscape shall consist of paths, lawns and trees, formally disposed. Squares shall be located at the intersection of Thoroughfares or Internal Drives and neighborhoods. The minimum size shall be 1/2 acre and the maximum shall be 5 acres.

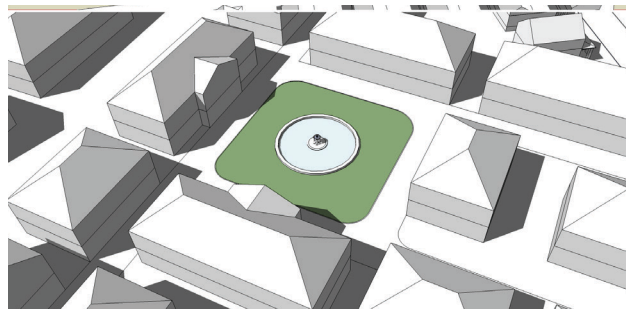
### Specifications

Size	1/2 ac. min., 5 ac. max.
Frontage	Buildings; 10% of Square must Enfront a Thoroughfare
Character	Formal

### Typical Facilities

- Passive recreation
- Paths
- Accessory structures

## Plaza



### Permitted In:

CD-4 SD-AE\*

### Intent

An Open Space available for Civic purposes and Commercial activities. A Plaza shall be spatially defined by Building Frontages. Its landscape shall consist primarily of pavement. Trees are optional. Plazas should be located at the intersection of important Thoroughfares or Internal Drives. The minimum size shall be 1/4 acre and the maximum shall be 2 acres.

### Specifications

Size	1/4 ac. min. 2 ac. max.
Frontage	Buildings; 10% of Plaza must Enfront a Thoroughfare
Character	Formal

### Typical Facilities

- Passive recreation
- Paths
- Accessory structures
- Water features

\*As indicated in Table 10-I.4.2.1



# SECTION 10-I.4: STANDARDS & REQUIREMENTS

TABLE 10-I.4.12.2B CIVIC SPACE – SPECIFIC STANDARDS

## Playground



### Permitted In:

**CD-1 CD-3 CD-4 SD-AE\***

#### Intent

An Open Space designed and equipped for the recreation of children. A Playground should be fenced and may include an open shelter. Playgrounds shall be interspersed within Residential areas and may be placed within a Block. Playgrounds may be included within Natural Areas and Greens. There shall be no minimum size and the maximum size shall be 2 acres.

#### Specifications

Size	No min. 2 ac. max.
Frontage	Buildings; 10% of Playground must Enfront a Thoroughfare
Character	Formal or Informal

#### Typical Facilities

- Active recreation
- Play structures
- Paths
- Water features

## Sport Field or Court



### Permitted In:

**CD-3 CD-4 SD-AE\***

#### Intent

An open area designed and equipped for team sports activities.

#### Specifications

Size	No min. No max.
Frontage	Buildings; 10% of Sport Field or Court must Enfront a Thoroughfare
Character	Formal

#### Typical Facilities

- Active recreation
- Play structures

\*As indicated in Table 10-I.4.2.1

# SECTION 10–I.4: STANDARDS & REQUIREMENTS

TABLE 10–I.4.12.2B CIVIC SPACE – SPECIFIC STANDARDS

## Community Garden



### Permitted In:

CD-3 CD-4 SD-AE\*

### Intent

A grouping of garden plots available for small-scale cultivation, generally to residents without private gardens. Community Gardens should be fenced and accommodate individual storage sheds. Running water is required. Community Gardens shall be interspersed within Residential areas and may be placed within a Block or included within Natural Areas and Greens. There shall be no minimum or maximum size.

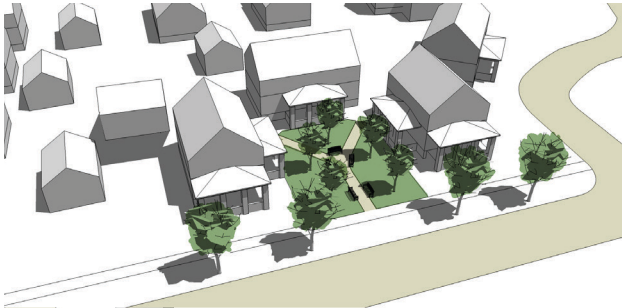
### Specifications

Size	No min.
	No max.
	If larger than 2 acres, a Pedestrian Path is required.
Frontage	May be Enfronted by Buildings or be located behind Buildings; 10% of Community Garden must Enfront a Thoroughfare
Character	Formal

### Typical Facilities

- Active recreation
- Garden plots
- Accessory structures
- Running water

## Pocket Park



### Permitted In:

CD-3 CD-4 SD-AE\*

### Intent

A small predominantly green open area available for unstructured passive recreation.

### Specifications

Size	500 sq. ft. min. 1/2 ac max.
Frontage	Independent; 10% of Pocket Park must Enfront a Thoroughfare
Character	Formal

### Typical Facilities

- Passive recreation
- Community gardens
- Playgrounds and play Structures
- Paths and trails
- Limited transient commercial concessions

\*As indicated in Table 10–I.4.2.1

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

### 10-I.4.12.3 Civic Space Standards.

1. All Lots, Building Sites, Development, Buildings, Structures, and Improvements within Civic Spaces must conform to and shall be designed as generally described for in **Table 10-I.4.2.1 (District & Civic Space Standards)**, as applicable; provided that any Civic Building shall be located within a Civic Space or at the axial termination of a significant right-of-way. The particulars of the design of each Civic Building shall be determined by the Legislative Body.
2. Except for Playgrounds, each Civic Space shall have a minimum of 10% of its perimeter Enfronting a Thoroughfare.

### 10-I.4.12.4 Reservation for Civic Purposes.

Civic Buildings and Civic Spaces shall be permanently reserved for Civic purposes, and shall be either owned by the Town or owned by a Property Owner's Association, Homeowner's Association, or land conservancy and made subject to an easement that grants to the public the right to use the Civic Space for Civic purposes.

## SECTION 10-I.4.13

### PRIVATE & PUBLIC LANDSCAPING.

#### 10-I.4.13.1 General.

1. Lot or Building Sites shall be landscaped in accordance with **Table 10-I.4.2.1 (District & Civic Space Standards)** and this Section 10-I.4.13.
2. Additionally, Civic Spaces shall be designed as set forth in **Table 10-I.4.12.2B (Civic Space – Specific Standards)**.
3. All Public Landscaping shall comply with this Section 10-I.4.13.

#### 10-I.4.13.2 Plant Materials.

Except as otherwise required by Sections 10-I.4.13.4 and 10-I.4.13.5, all plant materials shall meet with the minimum container size, class, and other requirements outlined in American Standards for Nursery Stock (ANSI

Z60.1-2004) published by the American Nursery and Landscape Association (ANLA) or other local Nursery Association Standards.

Invasive species are prohibited. Invasive species along all Thoroughfares shall be removed and be replaced with non-invasive vegetation. At least 66% of added plant materials shall be native species.

#### 10-I.4.13.3 Placement of Trees.

Landscaping within the Public Frontage and landscaping within Civic Spaces shall comply with the following standards:

- a. The same shall be centered horizontally and placed minimally:
  - (1) Two (2) feet from walkways, Curbs, and other impervious surfaces if planted in a tree well or continuous planter;
  - (2) Three (3) feet from walkways, Curbs, and other impervious surfaces if planted in a continuous swale or uncontained area.
- b. Five (5) feet from Thoroughfare lights, utility meters and service lines, Fences, walls, and other ground level obstructions;
- c. Six (6) feet from porch eaves, awnings and similar overhead obstructions associated with the ground level of Buildings; and
- d. Eight (8) feet from balconies, verandas, Building eaves and cornices, and similar overhead obstructions associated with the upper Stories of Buildings.

#### 10-I.4.13.4 Tree Size.

At installation, trees shall be a minimum of two and a half inches (2.5") in caliper, measured three feet (3') above the ground, and a minimum of ten feet (10') tall.

#### 10-I.4.13.5 Shrub Size.

At installation, Shrubs shall be a three (3) gallon container minimum and a height of 30 inches (30")

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

minimum. Shrubs shall be 18" minimum clear from any Sidewalk or pavement edge at the Lot or Building Site Line.

### 10-I.4.13.6 Bare / Exposed Ground.

All bare or exposed ground on the site and in landscaped areas shall be covered with live plant materials and/or mulch, with the following exceptions:

- a. Naturally occurring creek beds, rock outcroppings or similar landscape features typically lacking in vegetation;
- b. Hiking trails and/or traces;
- c. Clay or sand surfaces associated with recreation fields and facilities.

### 10-I.4.13.7 Artificial Plants / Turf.

Artificial plants and artificial turf are prohibited.

### 10-I.4.13.8 Temporary Spray Irrigation.

Temporary spray irrigation systems may be used to establish seeded areas for grass and groundcover and native drought-tolerant landscape.

### 10-I.4.13.9 Water Features.

Constructed water features such as fountains, streams and ponds that operate with water recirculation systems shall be designed to prevent seepage and leaks.

### 10-I.4.13.10 Prevention of Compaction.

The soil structure of planting strips shall be protected from compaction with a temporary construction Fence. Standards of access, excavation, movement, storage and backfilling of soils in relation to the construction and maintenance of deep utilities and manholes shall be specified.

### 10-I.4.13.11 Preservation of Topsoil.

The topsoil within the area of disturbance within a Development Site or construction area shall be removed, stored and amended as recommended by a landscape soils test.

### 10-I.4.13.12 Mitigation of Wind Erosion.

Wind erosion shall be mitigated and controlled through dust abatement and similar practices during the period of site work and construction.

### 10-I.4.13.13 Compacted Soils.

Landscape soils that have been compacted during construction activities shall be loosened and aerated to a depth of at least six inches (6") before planting.

### 10-I.4.13.14 Pavement of First Layer.

In CD-1, CD-4, SD-AE, and CS, the First Layer shall not be paved, except for the following:

1. Driveways; and
2. Pavement matching the adjoining Public Frontage in Character District CD-4 where the Building is set back no more than five (5) feet from the Front Lot Line.

### 10-I.4.13.15 Condition of Plants.

All planted vegetation shall be living or dormant, and shall be maintained to preserve health as determined by species for at least two (2) planting seasons, or two (2) years, whichever is longer. Any planted vegetation that becomes diseased or dies within such period shall be replaced.

### 10-I.4.13.16 Structural Soil.

Structural soil shall be utilized adjacent to tree pits.

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

### SECTION 10-I.4.14 SUSTAINABILITY.

#### 10-I.4.14.1 Sustainability Methods.

a. Principal Buildings that incorporate eight (8) or more of the Energy Demand Reduction Methods and at least one (1) of the Renewable Energy Generation Methods set forth in **Table 10-I.4.14.1A (Sustainability Methods)** shall be entitled to a one (1) Story Height bonus for the Principal Building above the Height otherwise permitted in the applicable District.

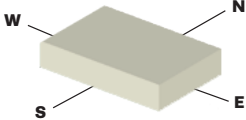
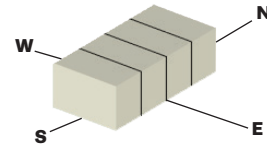
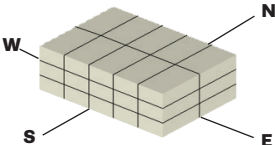


**TABLE 10-I.4.14.1A SUSTAINABILITY METHODS**

	CD-1	CD-3	CD-4	SD-AE	CS
<b>METHOD: ENERGY DEMAND REDUCTION</b>					
Surface-to-Volume Ratio pursuant to <b>Table 10-I.4.14.1B (Surface-to-Volume Ratio &amp; Building Orientation)</b>	NA	P	P	P	P
Building Orientation pursuant to <b>Table 10-I.4.14.1B (Surface-to-Volume Ratio &amp; Building Orientation)</b>	NA	P	P	P	P
Glazing to provide a direct line of sight to glazing from ninety percent (90%) of all regularly occupied spaces	NA	P	P	P	P
75% Operable Windows	NA	P	P	P	P
Shading of Glazing for all south-facing windows pursuant to <b>Table 10-I.4.14.1C (Shading of Glazing)</b>	NA	P	P	P	P
Daylighting pursuant to Section 10-I.4.14	NA	P	P	P	P
High Albedo Roofs, with (a) Solar Reflective Index [SRI] of $\geq 30$ for sloped roof or $\geq 78$ for flat roof or (b) SRI $\geq 10\%$ higher than the SRI required by the Maine Building Code, whichever is greater	NA	P	P	P	P
High Albedo Pavement, with (a) SRI of $\geq 30$ or (b) SRI $\geq 10\%$ higher than the SRI required by the Maine Building Code, whichever is greater	NA	P	P	P	P
Green Roof covering at least 75% of total roof area	NA	P	P	P	P
<b>METHOD: RENEWABLE ENERGY GENERATION</b>					
Small Windmill	NA	P	P	P	P
Roof-Mounted Photovoltaic Solar Energy System generating $\geq 4$ kilowatts or more of energy	NA	P	P	P	P
Geothermal System providing all Building heat and hot water needs	NA	P	P	P	P

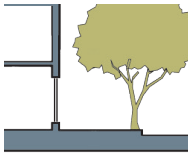
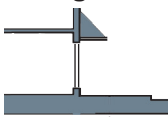
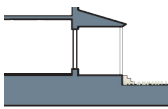
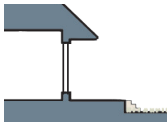
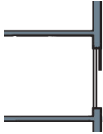
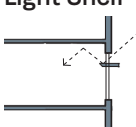


LEGEND  Permitted NA Not Applicable

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

**TABLE 10-I.4.14.1B SURFACE-TO-VOLUME RATIO & BUILDING ORIENTATION**

TYPE OF BUILDING	CD-1	CD-3	CD-4	SD-AE	CS
<b>Single-Unit one story</b>  S/V Ratio: High Orientation: E-W	NP	P	P	P	P
<b>Side-by-Side Units</b>  S/V Ratio: Medium Orientation: N-S	NP	NP	P	P	P
<b>Multi-unit</b>  S/V Ratio: Low Orientation: E-W	NP	NP	P	P	P
<b>LEGEND</b>  Permitted  Not Permitted					

**TABLE 10-I.4.14.1C SHADING OF GLAZING**

METHOD	CD-1	CD-3	CD-4	SD-AE	CS
<b>Tree</b> 	NA	P	P	P	P
<b>Awning</b> 	NA	NP	P	P	P
<b>Porch</b> 	NA	P	P	P	P
<b>Roof Overhang</b> 	NA	P	P	P	P
<b>Exterior Shade</b> 	NA	NP	P	P	P
<b>Light Shelf</b> 	NA	NP	P	P	P
<b>Deep Windows</b> 	NA	P	P	P	P
<b>Double Skin</b> 	NA	NP	NP	NP	P



## SECTION 10-I.4: STANDARDS & REQUIREMENTS

b. Any Roof-mounted Solar Energy System panels shall comply with the following standards in CD-3:

(1) The total square footage of such panels shall not exceed the total area of roof surface of the Building to which the Solar Panels are attached.

(2) Solar Panels shall be parallel to the roof surface.

(3) Except in CD-3, Solar Panels may only be located in the Third Layer.

(4) No signage or writing of any kind is permitted on any portion of a Solar Panel, other than required manufacturer plates and safety labeling.

c. Any Ground-mounted Solar Energy System panels shall comply with the following standards:

(1) Unless located on a Parking Structure, Freestanding Solar Panels shall be located in the Third Layer and meet Accessory Building Setbacks.

(2) Unless located on a Parking Structure, maximum Height shall not exceed the allowable Height of an Accessory Building, in the District.

(3) All panels must be constructed of non-reflective materials, or treated with an anti-reflective material.

(4) Freestanding Solar Panels must be encircled by a security fence or wall a minimum of six feet (6') tall, with a vegetative Screen provided outside the security fence meeting the Screening standards of this Article.

### 10-I.4.14.2 Public Darkness.

Within all Districts, all exterior lighting shall conform to the shielding, brightness and curfew standards defined in **Table 10-I.4.14.2 (Public Darkness)**.

**TABLE 10-I.4.14.2 PUBLIC DARKNESS**

	CD-1	CD-3	CD-4	SD-AE	CS
<b>Ambient Light Levels</b>	none	very low	low	low	low
<b>STANDARDS</b>					
<b>Maximum Lighting Standards</b>	Minimal electric lighting; should be turned off most of the time	Minimal lighting, all Full Cutoff, controlled with motion sensors	Full Cutoff lighting, controlled with dimmers, time switch or motion sensors	Full Cutoff lighting, controlled with dimmers, time switch or motion sensors	Full Cutoff lighting, controlled with dimmers, time switch or motion sensors
<b>Min/Max Lighting Level @ Frontage Line</b>	Per Table 10-I.4.10.2	Per Table 10-I.4.10.2	Per Table 10-I.4.10.2	Per Table 10-I.4.10.2	Per Table 10-I.4.10.2
<b>Required Shielding</b>	Fully shielded Luminaire with no uplight, or better	Fully shielded Luminaire with no uplight, or better	Shielded Luminaire or better	Shielded Luminaire or better	Shielded Luminaire or better
<b>Lighting Curfew for Non-Residential</b>	NA	NA	10pm or close of business, whichever is later	10pm or close of business, whichever is later	NA

## SECTION 10-I.4.15

### WORKFORCE AFFORDABLE HOUSING.

#### 10-I.4.15.1 Requirement.

Any proposed Development that consists of ten (10) or more Dwelling Units shall include at least ten percent (10%) of the total number of Dwelling Units within the Development as Workforce Affordable Housing.

#### 10-I.4.15.2 Height Bonus.

A one (1) Story Building Height bonus above the applicable maximum Building Height otherwise permitted in the applicable District shall be allowed per Principal Building in a Development that consists of ten (10) or more Dwelling Units, if such Development includes at least twenty-five percent (25%) of the total number of Residential Units within the Development as Workforce Affordable Housing. Such Building Height

## SECTION 10-I.4: STANDARDS & REQUIREMENTS

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Bonus shall not result in more than three (3) total Stories in CD-3 and four (4) total Stories in CD-4.

### 10-I.4.15.3 Calculations.

Calculations under this Section 10-I.4.15 shall be rounded to the nearest whole number.

### 10-I.4.15.4 Additional Requirements for Workforce Affordable Housing.

The additional following requirements shall be applicable to Workforce Affordable Housing under this Article 10-I:

- a. All Workforce Affordable Housing must comply with all standards and requirements applicable to non-Workforce Affordable Housing Development and Buildings.
- b. Workforce Affordable Housing units shall be integrated within the design of the applicable Development and Building and not be physically located away from non-Workforce Affordable Housing units.
- c. The exterior appearance of Workforce Affordable Housing units shall be indistinguishable from non-Workforce Affordable Housing units.
- d. The governance standards specified in Section 10-F.6 shall be followed for Workforce Affordable Housing Dwelling Units, and this requirement shall be noted in the approved plan and Findings of Fact.
- e. Any unit of Workforce Affordable Housing that is offered for sale shall be limited in its maximum resale price. The total resale price must not exceed the percentage of the property's fair market value (as determined by an independent real estate appraiser) that the seller paid for the property at the time of his or her purchase. For example, if the seller paid \$200,000.00 for a Workforce Affordable Home whose fair market value was determined to be \$250,000.00 (that is, 80% of the fair market value), then the maximum resale price of the property, if appraised at \$300,000.00, would be 80% of that value, or \$240,000.00. Notwithstanding the above

calculation, the resale price must not exceed an amount that is affordable for households of Moderate Income.

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# APPENDIX A

## FORMULA FOR CALCULATING THIRTY-FIVE-PERCENT SHADING OF PARKING AREAS

The following is a formula for determining the number of shade trees required in and around paved Parking Lots in order to presumptively satisfy the shading requirements of Section 10-I.4.8.7.

FORMULA FOR CALCULATING 35% SHADING OF PARKING AREAS		
(1)	Including Parking spaces, Driveways, loading areas, Sidewalks, and other circulation areas and not including Building area or any area which will remain completely undeveloped, calculate square footage of the vehicle accommodation area:	_____ square feet
(2)	Multiply x 0.35:	_____
(3)	Area to be shaded:	_____ square feet
Add:		
(4)	Area shaded by existing trees to be retained in and around the vehicle accommodation area <sup>1</sup> :	_____ square feet
(5)	Area shaded by required Screening trees, if any <sup>1</sup> :	_____ square feet
(6)	Area shaded by required Street trees, if any <sup>1</sup> :	_____ square feet
(7)	Subtotal:	_____ square feet
[If line (7) is greater than line (3), then the shading requirement has been met. If not, go to line (8).]		
(8)	Enter the difference between line (7) and line (3):	_____ square feet
(9)	Divide line (8):	_____ / 707
(10)	Total number of shade trees required within the vehicle accommodation area:	_____ trees

<sup>1</sup>Existing trees retained will be credited according to their actual crown radius. Shaded area may be calculated as follows:  
 $3.14 \times (\text{crown radius})^2 = \text{shaded area}$

Trees planted within the vehicle Parking Area are credited with shading 707 square feet (based on a crown radius of 15 feet). New or existing trees on the perimeter, within 15 feet of the Parking Lot, are credited for having only half (½) a crown over the vehicle accommodation area (e.g., new perimeter trees will be credited for shading 354 square feet). When smaller trees, such as dogwoods, are planted, the credited shading area may be adjusted downward to 314 square feet for interior trees and 157 square feet for perimeter trees (based on a crown radius of 10 feet) by the Planning Board.